

OFFICIAL MAGAZINE FOR THE INTERNATIONAL YACHTING FELLOWSHIP OF ROTARIANS

ROTA FLOAT

JUNE 2017 ISSUE NO. 106



IC Jun at the wheel of Premier Dolphin Cruise Catamaran Portofino 1 at Pensacola Bay
The picture is symbolic of Jun's last helm command as our International Commodore



JESUS 'JUN' AVECILLA
INTERNATIONAL COMMODORE



I take great pride and joy for being the first Asian and Filipino International Commodore to lead the august Fellowship in its 70 years of existence, having taken over command from the first Italian IC and delightful to have passed the helm to the first Argentinian IC as well. The has been marked with firsts, relatively smooth and full of glorious events. Not that we did not encountered inclement weather along the way with cross currents and strong winds but the International Bridge Staffs are quick in exceptional seamanship and kept the Fellowship in steady course and safely navigating through the turbulent times. I have the privilege of working with a great Executive Committee that I do not recall having any major disagreement with on matters that need to be addressed and quickly decided upon. It would be an understatement to say that I fully enjoy the most harmonious working relationship. There has been no proposal to amend the General Rules during an IC term for the first time I guess in the history of our Fellowship if that is an indication how things well are.

A happy crew makes a happy boat and ultimately a winning boat as I constantly advocate to my racing crew whenever and wherever we race in any regattas. I kept the same guiding principle and personal mission in running our Fellowship. Thus, bridging the gaps of misunderstanding from the past was one of my focuses and has tried to mend that I think had yielded positive results and that eventually made everyone generally happy. It has been said that as sailors, Nelson's blood runs in our veins that we are brothers no matter what differences we have, the band of brotherhood prevails and we remain as one big family. From the continents of Asia, Australia, Europe to North and South America in countries where I visited whether to Charter new Fleets, attend Area General Meetings or Fellowship events, in spite of differences in language, religion, race and culture the bond of goodwill and friendship transcend and keep us together making our Fellowship unique.

I am pleased to leave our Fellowship with 3,685 members in 126 Fleets and 38 countries all over the world. For the first time in recent years IT Andre reported that "The slide of membership dues has been stopped and one can notice an increase in membership dues" despite a US\$20,000 grant has been given out this year to pursue various projects of the Fleets in the 3 Areas. Dramatic rise in memberships and new Fleets were seen in the Philippines Area 1 and South America Regions with the Walter Eaglehurt's laudable and pioneering development of Sea Scouts, first in Chile and now in Ecuador in of Area 2. Quite a number in Area 1 as well, all these because of the hard works of the Area and Region Commodores. Our newsletter Rotafloat has been transformed in to a news magazine with articles of various interests and serving as an archive of the recent events in the annals of our history to the reading pleasures of our wide readerships.

I extend my profound gratitude and whole-hearted thanks and pay tribute to all of my International Bridge without going through their names for they are listed in the succeeding pages herewith. Foremost to my Big Brother Chief of Staff, the Executive Committee, the Area and Regional Commands, International and Area Treasurers, Webmaster, Rotafloat Editor, Regalia Officers, Committee Chairmen, Officers and International Staffs and IC Special Assistants and Advisers and to all the Rotary Mariners. For the great honor and distinct privilege that you have given for me to serve the Fellowship is a treasure that I will always keep and remember from the day I took command in Sao Paulo to the day I have turned it over in Atlanta and Pensacola up to time I am called to sail my final voyage. It has been a blissful and memorable experience to have met and to have been acquainted with all of you my beloved crew, crossing the finish line happy together and WINNERS all!



Finally, I extend my best wishes to our new IC Guillermo Arteta and his EC, IVC Robert Burns and IRC David Hansen and his new International Staffs during their stint. I am confident that they will lead us to the next horizon in a higher bar. Let us all throw our support to all of them for their success.

Until we meet again in Alicante for the Area 1 GM in October, in Kobe for the Area 3 GM in November and in Chile for the Area 2 AGM in May next year.

Sail on and Mabuhay Tayong Lahat.





INTERNATIONAL BRIDGE 2015-2017

Jesus E. Avecilla (Philippines)

International Commodore

Guillermo Arteta (Argentina)

International Vice Commodore, A2C

Robert Burns (GB & I)

International Vice Commodore, A1C

Angelica Mesistrano (Uruguay)

Area2 Commodore

IC Special Assistant

Kerran Campbell (Austria)

Area 3 Commodore

Sergio Santi (Italy)

Immediate International Past Commodore

Chief of Staff

Maximiano R. Tan (Philippines)

IC Executive Assistant

Juan Paolo Villian Vioria (Philippines)

Fernando Khan (Argentina)

Assistant Webmaster

Vincenzo Rinaldi (Italy)

IC Special Assistant/

Adriatic Fleets Coordinator

Zedrik T. Avecilla (Philippines)

Administrative Officer

Joze C. Sison (Philippines)

Legal Advisor

Terry Stretton (New Zealand)

IC Special Assistant/Advisor

Andre Gorgemans (Switzerland)

International Treasurer

Roger Lingard (Australia)

Public Relations Officer Chair

PCC Affairs

Carlo d'Amore (Spain)

IC Special Assistant

Annete Lewis (GB & I)

Area1 Bank Administrator/Treasurer

Cesare Ortiz (Italy)

International Convention Chair 2015

Guia A. Ayeras (U.S.A.)

International Convention Chair 2016

Oyie Valarao (Philippines)

IC Special Assistant

David Hansen (New Zealand)

IC Special Assistant

George Futas (USA)

Area2 Bank Administrator/Treasurer

North America Region Commodore

Bryan Skinner (GB&I)

IYFR Grants Chairman

Koji Kajino (Japan)

IC Special Assistant

Jayson Herrero (Philippines)

Area3 Bank Administrator/Treasurer

Ken Winter (New Zealand)

Historian

Piotr Pajdowski (Poland)

IC Special Assistant

Pat Galligan (Australia)

IC Special Assistant

Luigi Norsa (Italy)

Web Administrator/Webmaster

Ann Lockhart (GB&I) Area 1

Christian Datol (Philippines) Area 2

Jayson Herrero (Philippines) Area 3

Regalia Officers

Grahame Werrell (Australia)

IC Special Assistant

Zeev Matar (Israel)

IC Special Assistant/Rotafloat Editor

IC Jesus"Jun" Avecilla and the International Bridge is welcoming on board

David Hansen elected IRC

from Auckland Fleet and South Pacific Region Commodore.

The assumption of the IRC command installation will take place in IYFR AGM convention in Atlanta



First Row: (L-R) Jesus Avecilla, Guillermo Arteta, Robert Burns, Angelica Mesistrano Kerran Campbell,
Second Row: Sergio Santi, Maximiano Tan, Zedrik Avecilla, Andre Gorgemans, Annette Lewis
Third Row: George Futas, Jayson Herrero, Luigi Norsa, Juan Paolo Villoria, Jose Sison,
Fourth Row: Roger Lingard, Cesare Ortis, Bryan Skinner, Ken Winters, Ann Lockhart
Fifth Row: Christian Datol, Zeev Matar, Vincenzo Rinaldi, Guia Ayeras, Carlo d'Amore
Sixth Row: Gloriosa Valarao, Terry Stretton, David Hansen, Kozo Kajino, Piotr Pajdowski
Seventh Row: Pat Galligan, Grahame Werrell

TABLE OF CONTENTS

| | |
|---|-----|
| <u>INTERNATIONAL COMMODORE'S MESSAGE</u> | 1 |
| <u>INTERNATIONAL BRIDGE 2015-2017</u> | 3 |
| <u>INTERNATIONAL IYFR EVENTS YEAR 2017</u> | 6 |
| <u>IYFORIANS POEM IN ENGLISH</u> | 8 |
| <u>ATLANTA ANNUAL GENERAL MEETING & POST CONVENTION CRUISE:</u> | |
| <u>GREET AND MEET DINNER</u> | 9 |
| <u>ANNUAL GENERAL MEETING</u> | 12 |
| <u>GALA DINNER</u> | 15 |
| <u>IYFR BOOTH AND CHILE SEA SCOUTS BOOTH</u> | 21 |
| <u>BUS TRAVEL FROM ATLANTA TO PENSACOLA</u> | 25 |
| <u>DAYS IN PENSACOLA</u> | 26 |
| <u>AREA 1 – ANNUAL GENERAL MEETING (SPAIN)</u> | 36 |
| <u>ALICANTE – Notes about the City</u> | 40 |
| <u>IYFORIANS POEM FOR GREECE</u> | 42 |
| <u>IYFORIANS POEM FOR ITALY</u> | 43 |
| <u>AREA 1 NEWS:</u> | |
| <u>IYFORIANS POEM FOR POLAND</u> | 44 |
| <u>POLAND – GIZYCKO FLEET</u> | 45 |
| <u>POLAND – ORC SAILING CHAMPIONSHIP</u> | 49 |
| <u>IYFORIANS POEM FOR GERMANY</u> | 52 |
| <u>POLAND - POMERANIA FLEET</u> | 53 |
| <u>IYFORIANS POEM FOR SLOVENIA</u> | 55 |
| <u>SLOVENIA – SLOVENIA FLEET</u> | 56 |
| <u>SLOVENIA – SAILING FOR TOMORROW</u> | 61 |
| <u>IYFORIANS POEM FOR SPAIN</u> | 63 |
| <u>IYFORIANS POEM FOR TURKEY</u> | 64 |
| <u>AREA 1 - GEOGRAPHICAL SEA SITES</u> | |
| <u>GIBRALTAR STRAIT</u> | 65 |
| <u>AREA 2 NEWS:</u> | |
| <u>IYFORIANS POEM IN SPANISH</u> | 75 |
| <u>ARGENTINA – Rio de la Plata Fleet</u> | 76 |
| <u>CHILE – PUERTO VARAS SEA SCOUTS</u> | 78 |
| <u>COLUMBIA – CARTAGENA DE INDIAS FLEET</u> | 82 |
| <u>AREA 2 – GEOGRAPHICAL SEA SITES</u> | |
| <u>BERING STRAIT</u> | 86 |
| <u>IYFORIANS POEM FOR JAPAN</u> | 98 |
| <u>AREA 3 – ANNUAL GENERAL MEETING (JAPAN)</u> | 99 |
| <u>AREA 3 NEWS:</u> | |
| <u>IYFORIANS POEM FOR PHILIPPINES</u> | 102 |
| <u>PHILIPPINES REGION</u> | 103 |
| <u>GLOSSARY OF NAUTICAL AND SAILING TERMS</u> | 105 |
| <u>ADVERTISING</u> | 116 |
| <u>EDITOR'S NOTE</u> | 120 |
| <u>IYFORIANS POEM FOR THE ISRAELI FLEET</u> | 123 |
| <u>SAILOR SONGS</u> | 124 |
| <u>SAILING SONGS</u> | 125 |
| <u>PHOTOGRAPHY</u> | 127 |
| <u>HISTORICAL TALL SHIPS – KRUZENSHTERN 1927</u> | 132 |

Please click on the title to open and read the article that interests you.



About the Cover

IC Jun at the wheel of Premier Dolphin Cruise Catamaran Portofino 1 at one of the PCC's great activities watching the Dolphins at Pensacola Bay. The picture is Symbolic of Jun last helm as IC of the Fellowship. Next to Jun Rotafloat Editor Zeev talking with the Cat skipper at the background.



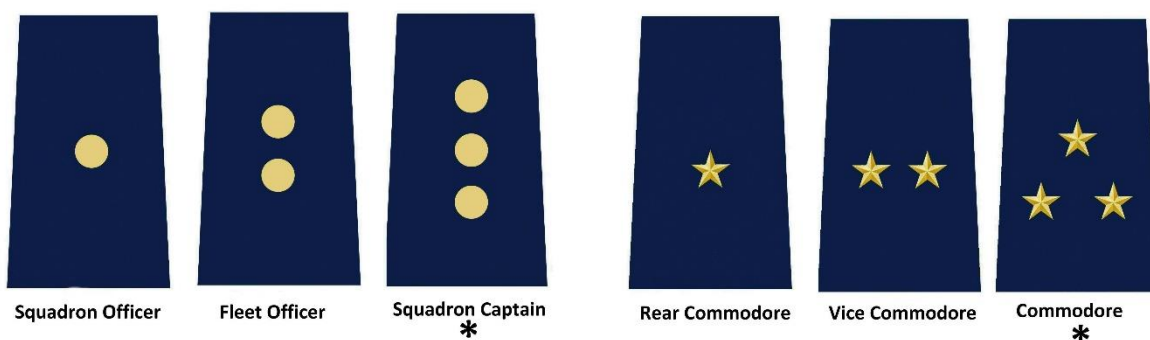
IYFR Philippines

International Yachting Fellowship of Rotarians



**We present the updated IB Epaulettes to be effective starting on 1st May 2017
as per decision of the Executive Committee (EC)**

SQUADRON FLEET BRIDGE



INTERNATIONAL BRIDGE EC



From IYFR bylaw



**International Yachting
Fellowship of Rotarians**

**IYFR Flags – Uniforms
&
Boating Practice**

[Click to Open](#)

Regalia

International Yachting Fellowship of Rotarians



IYFR LADY'S SCARF

Our IYFR men have had the IYFR tie to wear for many years now, but there has never been anything corresponding for the Ladies. Members have often said that there should be an attractive lady's scarf available. Well now one has finally been produced by our friends in France.

The scarf is beautiful white pure silk with Rotary blue and yellow edging and IYFR logos attractively placed throughout, as shown in the pictures. A scarf of such high quality will be greatly appreciated by our ladies; I have no doubt, and will be worn with pride not only at many IYFR and Rotary functions, but at other elegant events.

It is very reasonably priced, for such a quality scarf, at 60.00 Euros plus postage. Anyone who is interested in purchasing one should contact our Area 1 Regalia Officer, Ann Lockhart, at ann.lockhart33@live.com, who will be delighted to provide you with one.





International Yachting Fellowship of Rotarians



Dedicated poem to our Fellowship

Iyforians poem

Yachting Fellowship of Rotarians
We are the kings of seas
Crossing any lake or ocean
With blowing sails by wind
We are all united Fleets
Spread under different Burgees
In any kind of country
Making love with all the seas
Go on sailing amigos
With open sails of friendship
Brotherhood is our language
Vodka and Champaign
Are our eternal trophies
Wine and Rum
Is our blood in veins to run
Viva and Aisa
Mabuhay to all of you my friends
Peace to all of you Iyforians
Shalom is our word, Amen
Go on sailing mariners
Win for us sailing cups
Yachting is our common love
We are devoted sailors
In cloudy weather or shining sunlit
Go on sailing
Mariners and officers
Let's have infinite fun
Viva Yachting Fellowship of Rotarians
Let's move forward the wheel
Blessed by Poseidon the Greek
To challenge our yachts and the winds
And make us always win.

Zeev

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant





INTERNATIONAL IYFR EVENTS



Atlanta Annual General Meeting 9-15 June 2017



Pensacola, Florida - Post Convention Cruise 15-19 June 2017

Greet & Meet Dinner

Saturday 10.6.2017

Dantanna's - Downtown Atlanta

Our fellowship had been gathered for a very pleasant evening headed by our International Commodore Jun and his wife Lil, toasting and singing with high spirit of joy and fun







Pictures credits:

Douglas Hsia

Lila AVECILLA

Zeev Matar



Tali Kuper - Hava Nagila



Hyelmot Lotti - Shalom Aleichem



Hava Nagila Texas Style



Andre' Riew Hava Nagila



*Hava Nagila - Jednego Serca
Jednego Ducha 2010*



IL Divo - Hallelujah



*Bryan Adams & Luciano Pavarotti-
O Sole Mio*



Leonard Cohen - Hallelujah



Annual General Meeting

13.6.2017



Our meeting held on Tuesday in the Tallahassee Room, JW MARRIOTT Hotel had gathered the International Bridge at command of IC Jesus Jun Avecilla, as well as IVC, IRC and all other authorities.
The Event had started with a short prayer and commemoration of friends who had left us





AGENDA

1. CALL TO ORDER
2. APPOINTMENT OF MEETING CHAIRMAN
3. INVOCATION IRC/A1C BOB BURNS
4. WELCOME IVC GUILLERMO ARTETA
5. AREA REPORTS
 - AREA 1 A1C BOB BURNS
 - AREA 2 A3C ANGIE MESISTRANO
 - AREA 3 A3C KERRAN CAMPBELL
6. WEB MASTER REPORT
7. ROTAFLOAT EDITOR
8. TREASURER
9. IC REPORT
10. OTHER MATTER
11. ADJOURNMENT



IPIC SERGIO SANTI
CHIEF OF STAFF

A1C BOB BURNS

Area 1 Report

Please Click to view

A2C ANGIE MESISTRANO

Area 2 Report

Please Click to view

A3C KERRAN CAMPBELL

Area 3 Report

Please Click to view

RC Italy LUIGI NORSA

Web Master Report

Please Click to view

IC Assistant ZEEV MATAR

Rotafloat Editor Report

Please Click to view

IC JESUSE JUN AVECILLA

Report

Please Click to view



Article & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant



[Click to go back to Table of Contents](#)

Gala Dinner

13.6.2017

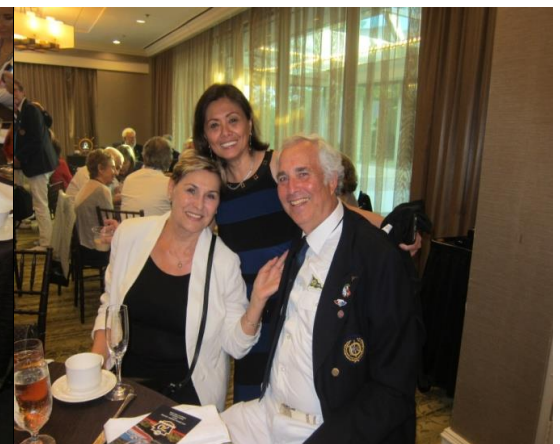
Change of watch ceremony



The gala dinner had gathered our Fellowship in the JW MARRIOTT hotel for happy hours of friendship















IYFR Booth & Chile Sea Scouts Booth



Atlanta Conventions Center

Both booths standing one in front of the other had been of a great interest for the enormous Rotarians public gathered from wherever around our globe

IYFR Booth

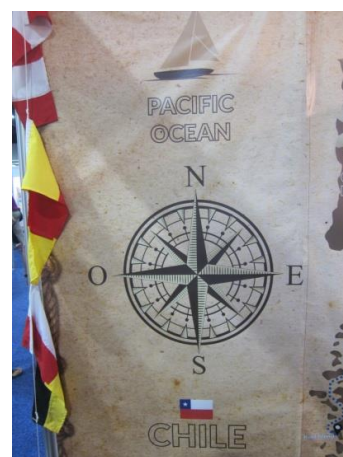
The IYFR booth had shown the oldest Fellowship within the Rotary, having celebrated 70 years of activity. Our people in charge of the booth had been very active to explain our scope as Fellowship, taking information about possible new Rotarians willing to join Fleets.



Chile Sea Scout Booth

Splendid group of 5 Sea Scouts between 12-17 years old headed by RC Walter Eaglehurst who not only developed the idea of the Sea Scouts, but the person who takes so much care for the Sea Scouts as if caring for his own family, with respect and loving smile.

The Sea Scouts booth was as last year in Korea the most visited booth in the ground floor being the real fresh attraction meeting the fabulous youngsters. The Sea Scouts had gained respect while paying honors to the visitors, who asked to be pictures with the youngsters.







Article & Layout
Zeev Matar



Rotafloat Newsletter Editor / IC Special Assistant

You Tube

Trailer Scouts Marinos IYFR





Bus travel from Atlanta to Pensacola

On the 18th of June 09.30 our group was attending in front of the JW Marriot in Atlanta to the pre ordered luxurious bus to take our Fellowship members to the PCC in Pensacola. The Bus which had arrived was far to be comfortable, but mariners don't cry so we raised anchor to go with the wind and with the force of engine to meet and greet our friends from Emerald bay Fleet and Mobile Bay Compass Rose Fleet.

Our distinguished Pilipino "reverend" CP William "Willy" Rogando had read to all of us a compassionate Tourist's prayer written by Arthur "Art" Buchwald who was an American Jewish humorist best known for his long-running columns in The Washington Post. Art died in the year 2007.

The Tourists' Prayer

Heavenly Father, look down on us, y6our humble obedient tourist servants who are doomed to travel this earth, taking photographs, mailing post cards, buying souvenirs and walking around in drip-dry underwear.

We beseech you, oh Lord, to see that our plane is not hijacked, our luggage is not lost and our overweight baggage goes unnoticed.

Protect us from surly and unscrupulous taxi drivers, avaricious porters and unlicensed, English-speaking guides.

Give us this day divine guidance in the selection of our hotels that we may find our reservations honored, our rooms made up and hot water running from the taps (if it is at all possible).

We pray that the telephones work and that the operators speak our tongue and that there is no mail waiting from our children which would force us to cancel the rest of our trip.

Lead us, Lord, to good, inexpensive restaurants where the food is superb, the waiters friendly and the wine included in the price of the meal.

Give us the wisdom to tip correctly in currencies we do not understand. Forgive us for under-tipping out of ignorance and over-tipping out of fear.

Grant us the strength to visit the museums, the cathedrals, the palaces and the castles listed as "musts" in the guide books. And if, perchance, we skip an historic monument to take a nap after lunch, have mercy on us for our flesh is weak.

(This part of the prayer is for husbands.)

Dear God, keep our wives from shopping sprees and protect them from "bargains" they don't need or can't afford. Lead them not into temptation for they know not what they do.

(This part of the prayer is for wives.)

Almighty Father, keep our husbands from looking at foreign women and comparing them to us. Save them from making fools of themselves in cafes and nightclubs. Above all, please do not forgive them their trespasses for they know exactly what they do.

(Together.)

And when our voyage is over and we return to our loved ones, grant us the favor of finding someone who will look at our home movies and listen to our stories, so our lives as tourists will not have been in vain.

Amen.



Our days in Pensacola with our Fellowship friends 15-19 June 2017 Emerald Bay Fleet & Mobile Bay Compass Rose Fleet

A group of Iyforians from the two organizing local Fleets had attended our bus arrival on Thursday 15 June to Pensacola Grand Hotel, this was our hotel during the Post Convention Cruise

On behalf of our Iyforian group coming for the Post Convention Cruise from Atlanta, we wish to thank our Fellowship brothers and sisters from Pensacola who had been part of the Organization committee as by the following:

TJ Goulet from Emerald Coast, Fleet member with the role of Event Captain – Closing Dinner
 Kimberly Herold from Emerald Coast, Fleet member with the role of Co-Captain - Transportation
 Jerry Jackson from Compass Rose, Past Commodore with the role of Event Captain – Dolphin Cruise
 Olevia McNally from Emerald Coast, Fleet member with the role of Event Captain – Naval Air Station
 Maria Mendez from Compass Rose, Fleet member with the role of Transportation and Event Assistance
 Staci Miron from Emerald Coast, Fleet Secretary with the role of Captain – Captain
 Gary Moore from Compass Rose, Regional Commodore with the role of PCC Coordinator
 Dan Sulger from Emerald Coast, Fleet Commodore with the role of PCC Coordinator/Co-Captain Transport
 Innes Richards from Emerald Coast, Fleet member with the role of Event Captain – Gala and Bay Cruise
 Paul Stanley from Compass Rose, Fleet Commodore with the role of Captain - Lodging



Friday 16/6/2017 Sailing with Catamaran Portofino 1 in Pensacola Bay



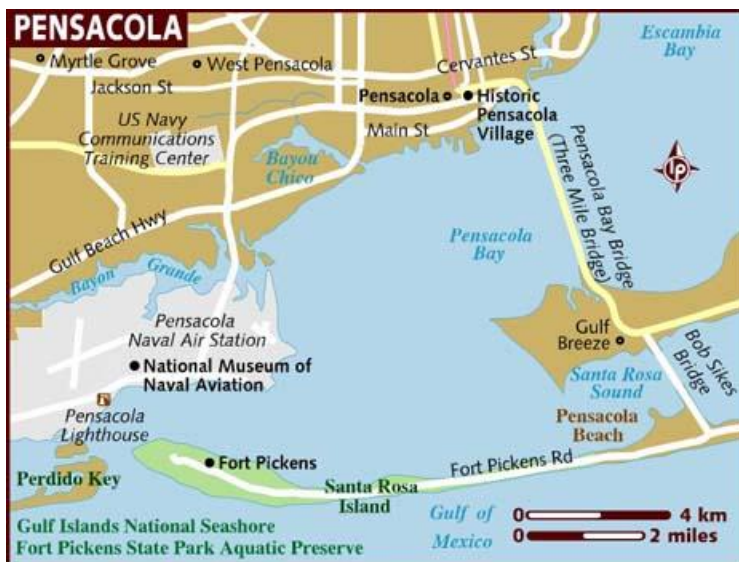


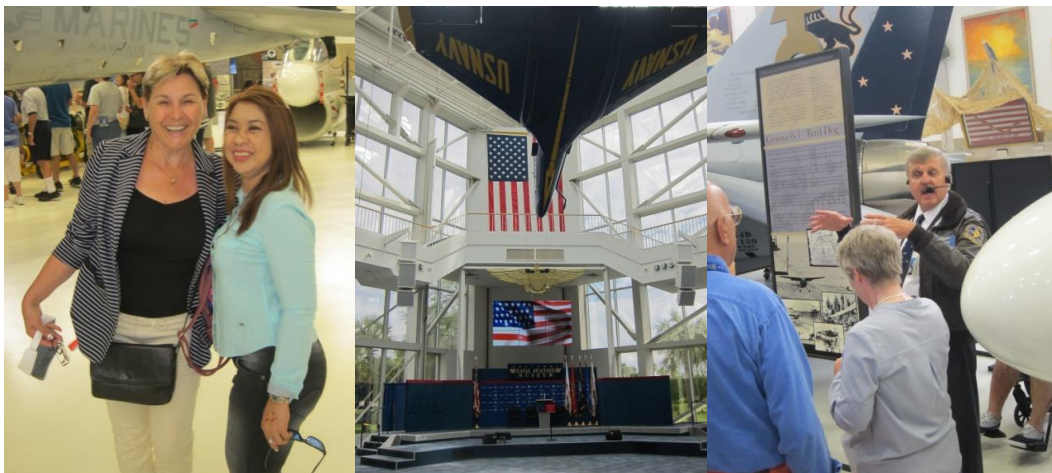




Saturday 17/6/2017 National Naval Aviation Museum

The National Naval Aviation Museum, formerly known as the National Museum of Naval Aviation and the Naval Aviation Museum, is a military and aerospace museum located at Naval Air Station Pensacola, Florida







Sunday 18/6/2017 Sailing day





On behalf of our entire group sailing with the motor boat and sailing yachts our deep gratitude for that wonderful sailing gesture

Our thanks to the hosts

To the two power boat captains Rudy Bray and Ronald Bruce & Rudy's son Ronald & for the two sailboat captains Jim Oyler and Johnny Smith and wives Linda Oyler, Joanne Bruce, and Ruth Smith

You have donated us several hours of joy and fun



Sunday 18/6/2017 Gala Dinner



The voice – Jesus “Jun” Avecilla

Pensacola Professional Pictures Album
Greet & Meet + Pensacola Bay + Gala Dinner

Please click to enter

**IYFR Post Convention Cruise is ended
Each of us turns to take his way back home
But our friendship is here to remain
with us forever
Signed by our common hugs**



**This is our Fellowship to go with us with open sails
In our daily regatta life knowing that in each corner
of our globe there is a friend to attend and open his hands
and around each mariner and officer we can meet Fleet
members and feel as in our own Fleet
Joy and fun**

**ON BEHALF OF OUR FELLOWSHIP FRIENDS WHO
ARRIVED TO PENSACOLA WE WISH TO THANK OUR
FRIENDS FROM
EMERALD BAY FLEET & MOBILE BAY COMPASS ROSE FLEET
WHO HAD GIVEN THEMSELVES FOR THE SUCCESS OF THE
PROGRAMMED EVENTS A HUGE HUG**

Article & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant



IYFR

Area 1 Annual General Meeting

19th – 22nd October 2017 in Alicante, Spain



As you know, we will be celebrating the Area 1 Meeting in Alicante in October at the same time as the start of the 'Round the World Volvo Ocean Race'. This promises to be a most enjoyable meeting, with an excellent program, culminating with our being on the water to watch the start of the race



Next October, from the 19th to the 22nd, we will meet in Alicante for the Area 1 Annual General Meeting. We will see the start of the Volvo Ocean Race (VOR 2017)

<http://www.volvoceanrace.com/es/home.html> and know this land of sea and sun.



For the people arriving on Thursday the 19th, they will have to go to the Hotel Maya
<http://www.hotelalicantemaya.com/>



View from the hotel

At 19:00 we will take the bus to take a look to the Village of the VOR. Then we will take a walk to the Royal Regatta Yacht Club where we will have dinner at the Aldebaran Restaurant at 21:00.



Village of the VOR

Friday morning, at 10 o'clock we will take the bus to visit the VOR Village. 11:30 we will depart from here to Tabarca Island where we will walk around this old "prisoners island" and taste the typical fishmeal "caldero de Tabarca".



Tabarca Island and one of the gates of the old town

By 16:30 we will go back to Alicante to visit Santa Barbara Castle by half past five.



The castle from the water

After visiting the castle there will be time to rest at the hotel.
Then, at 21:30 dinner at the hotel.

Saturday morning, at ten, take the bus to visit the historical downtown of Alicante.
At 13:30 there will be a bus to La Cantera Restaurant <http://restaurantelacantera.com/>
After lunch, at 16:30 the bus will take you back to the hotel.

At 19:00 the buses will go to the Cruise Terminal where will be held the Business Meeting and the dinner with the Flamenco Show.



The Cruise Terminal

Sunday morning the big day. At half past nine the bus will take you to the Port to board a boat to see the start of the race. There will be an appetizer on board.



After the start and see the boats leave south west to the Atlantic Ocean we will go back to the port and then go to the Diurno Restaurant for lunch to close the 2017 Area 1 AGM.

Subscription for the AGM in Alicante

You can register for the meeting at www.rotariosmariners.com

Once you have completed your registration, please confirm it by sending a copy of the Bank Transfer to gilberto@asesoriamogica.com , indicating the first name and surname of the IYFORIAN as well as the Fleet name of membership.



IYFR Antonio Garcia Campillo Comodoro ING

Editing & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

Alicante

notes about the city



Coming to Area 1 AGM to Alicante is not only to pay tribute to our Spanish Fellowship friends from the Fleet of Alicante, but to share our friendship with IYFR Iberian Region



The province of Alicante is located in eastern Spain in the Autonomous Community of Valencia.

Alicante is located on the Mediterranean coast, in the region known as Costa Blanca; the province is bordered by Valencia in the north, Murcia in the south west and the Mediterranean Sea to the east.



It is the right occasion to draw some sentences about Alicante history which had been inhabited for over 7000 years while hunter-gatherers arrived from Central Europe between 5000-3000 BC. Greek and Phoenicians (ancient Semitic civilization living on the coastline of Eastern Mediterranean arrived to the area bt 1000 BC and had established small trading ports for trading iron and pottery. In the 3rd century BC Carthage & Rome invaded the Iberian Peninsula fighting each other to control the area. It was the Carthaginian general Barca to establish the fortified “white Mountain” where Alicante stands today.

Even though the Carthaginians ruled the area around Alicante, the Romans ruled Hispania Tarraconensis (one of the three Roman provinces in Hispania) for over 700 years. Alicante passed to be ruled by the Moors and in 1246 taken by Castilians and later passed to the Kingdom of Valencia in 1298.



Alicante City Hall



Luceros Square



The Independent



Explanada de España



Torre Provincial in La Rambla de Alicante



Basilica of St Mary



Mount Benacantil and Castle of Santa Bárbara



Castle of Santa Bárbara



Alicante city – tourist guide



A week in Alicante

Article & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant



[Click to go back to Table of Contents](#)

International Yachting Fellowship of Rotarians



Dedicated poem to our Fellowship in Greece

Το ποίημα των μελών στόλων
Οι θαλασσοκράτορες είμαστε
σε λίμνες και ωκεανούς,
με πανιά ανοιγμένα στον άνεμο.
Στόλοι ενωμένοι, όλοι μας,
εκτεινόμαστε κάτω από διαφορετικές σημαίες
σε κάθε χώρα
και σε σχέση αγάπης πάντα με όλες τις θάλασσες.
Συνεχίστε, φίλοι, τους πλόες
με ανοιγμένα τα ιστία της φιλίας.
Αδελφосύνη είναι η γλώσσα μας ,
Βότκα και Σαμπάνια
τα ακατάλυτα τρόπαιά μας,
Κρασί και Ρούμι
ρέει στις φλέβες μας.
Ζήτω και Aisa
Mabuhay σε όλους σας, φίλοι μου,
ειρήνη σε όλους σας, μέλη στόλων
Shalom ο λόγος μας, Αμήν.
Καλή συνέχεια στους πλόες σας, θαλασσοπόροι.
Κερδίστε έπαθλα ναυσιπλοΐας
Η πλεύση είναι η κοινή μας αγάπη
κι' εμείς αφοσιωμένοι ναύτες,
είτε με συννεφιασμένο καιρό είτε με λιακάδες.
Συνεχίστε τους πλόες σας,
ναύτες και αξιωματούχοι.
Ας είμαστε πάντα σε απέραντη ευφορία.
Ζήτω η πλεύση της ροταριανής συναδέλφωσης.
Ας κινήσουμε μπροστά τον τροχό.
Με την ευλογία του Έλληνα Ποσειδώνα
ας προκαλούμε τα σκαριά μας και τους ανέμους.
Και πάντα με επιτυχίες!

Zeev

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

Credit Poem translation by FC Katerina Kotsali-Papadimitriou



International Yachting Fellowship of Rotarians



Dedicated poem to our Fellowship in Italy

Poema Iyforiano

Yachting Fellowship of Rotarians
Siamo i sovrani dei mari
Attraversando ogni lago od oceano
Con le vele gonfiate dal vento
Siamo tutte le Flotte unite
Distribuite sotto differenti Guidoni
In ogni tipo di nazione
Amando tutti i mari
Navigate amigos
Con le vele spiegate dell'amicizia
Fratellanza è la nostra lingua
Vodka and Champagne
Sono I nostri eterni trofei
Vino e Rum
è il sangue che scorre nelle nostre vene
Le 'haim, Viva e Aissa
Mabuhay a tutti voi amici miei
Pace a tutti voi Iyforiani
Shalom è la nostra parola, Amen
Navigate mariners
Vinceteci regate
Lo Yachting è il nostro comune amore
Siamo marinai devoti
Nel tempo nuvoloso o nella luce del sole splendente
Navigate
Mariners e ufficiali
Divertitevi infinitamente
Viva la Yachting Fellowship of Rotarians
Spostate in Avanti il timone
Benedetti da Poseidone il Greco
a sfidare le nostre barche ed i venti
E farci sempre vincere I mari

Zeev

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

Credit Poem translation to Italy RC Luigi Norsa



International Yachting Fellowship of Rotarians



Dedicated poem to our Fellowship in Poland

Pesem iyforovcev

Yahting bratovščina Rotarijcev,
smo kralji morja,
prečimo vsako jezero ali ocean.
Z vetrom napolnimo jadra.
Vsi smo združeni v flote
porazdeljene pod različnimi zastavicami
v katerikoli koli državi.
Ljubimo se z vsemi morji.
Pojdite jadrat prijatelji,
z odprtimi jadri prijateljstva.
Bratstvo je naš jezik,
vodka in šampanjec
sta naši večni trofeji,
vino in rum
je naša kri v žilah, da delujemo.
Pozdravi Viva, Aisa in Mabuhay
vsem, ki ste moji prijatelji,
mir za vse vas Iyforijce.
Shalom je naša beseda, Amen.
Nadaljujte plovbo mornarji,
osvajajte za nas jadrane pokale.
Yachting je naša skupna ljubezen,
predani smo mornarji,
v oblačnem vremenu ali v sijočem soncu.
Nadaljujte plovbo,
mornarji in funkcionarji,
naj se naša zabava nikoli ne konča.
Naj živi Yachting bratovščina Rotarijcev
Poženimo naprej naše kolo,
blagoslovljeno od grškega Pozejdona,
da izzovemo naše barke in vetrove
in za vedno zmagamo.

Zeev

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

Credit Poem translation to Elżbieta Pulawska

POLAND

GIZYCKO FLEET



GIZYCKO REGATTA



18th May - 21st May 2017

Back to source

Zeev Matar – Rotafloat Editor sails again with us in Poland

Zeev together with the International Commodore June Avecilla visited us in Masuria in 2016. We sailed there. Masuria is a land of connected lakes in the north-eastern part of Poland. It is the Polish center of inland yachting. Zeev returned this May to attend the annual events of two Fleets: the Giżycko Fleet and the Polish-German Pomerania Fleet. Zeev was also attracted to Poland because of his family roots and the fact that he knew our language. As his first stop, Zeev attended a meeting of the Warszawa Belweder Rotary Club in Warsaw. We were all touched when he spoke about his emotions towards Poland. On their way to Masuria, Maciek Matwiejczuk took Zeev to Tykocin to see the famous synagogue. Before the regattas in Giżycko I invited my crew to train aboard my yacht on the Beldan lake. Beside Maciek Matwiejczuk and Zeev the crew also included Anna Czarczyńska – President of RC Warszawa Belweder. We sailed the southern part of the Great Masurian Lakes all night. Later, when Zeev and I recalled his entire stay in Poland he said that it had been one of the most exciting moments. Still that night, Zeev called Jun Avecilla in the Philippines asking why he was not with us on the rocking boat in the middle of the Masurian wilderness. Jun had to promise he would come next year and we would enjoy all night yachting together.

The regattas in Giżycko

This year, the Europa hotel in Giżycko hosted 18 crews. All in all, we counted that there were about 100 participants and accompanying persons. A large and dominating group were our colleagues from Lithuania.



The Lithuanian group of regatta's participants is growing larger by the year. As usual, Thursday night was the time of joyful welcomes and cheers by a bonfire. Joasia Dzienis – the hotel owner and member of RC Giżycko – also, as usual, saw to it that the crews were welcome with something more substantial. The organization was also traditionally looked after by Ewelina Jelec. The first day of the regattas on Friday started with the official opening attended by the local authorities. Then all crews sailed out onto the lake on the same Maxus 24 boats (7.2 m long with a cabin) provided by the organizers. We sail three short and one long run. Sunshine and wind were in our favor. Then, the Annual Convention of the Giżycko Fleet in the evening. This year the agenda included passing on of the insignia and election of Rear Commodore. The guest of honor was a well-known Polish regatta yachtsman, Olympic Games participant – Piotr Cichocki. Saturday, the second day of the competition, was also sunny, but the wind calmed down soon after the bugle call. We drifted a bit around the lake and then it was time to return to harbor. In contrast, the night after the results had announced and prizes handed over was wonderful. As always. A great surprise of this year's results was the second place won by Darek Dziedziul – from RC Gdańsk- Sopot – Gdynia, current Commodore of the Giżycko fleet. He usually won the race, but this year had to give in to Piotr Konstantynowicz's crew. A rematch next year.

After the regattas

Wind returned on Sunday. Other crews went back home. Our crew decided to continue the jaunt aboard the regatta boat. Ania, Zeev, Maciek and myself sailed north to Sztynort. The fish at the Fisherman's Daughter inn was an unforgettable feast. Sunday, the fourth day of the rally, regretfully marked the end of the annual event of Rotarian sailors in Giżycko.

However, Zeev decided to stay a few more days. Marek Stawicki and Uwe Schuetze from RC Giżycko took Zeev to Vilnius, the capital of Lithuania. For him, it was another encounter with the unusual Polish and Jewish history. Before WW2, Vilnius belonged to Poland and a significant part of its population was Jewish.

From Vilnius via Warsaw Zeev went to Szczecin to attend the Pomerania Fleet convention. It was another cultural change for him. In good hands of Jarek Wistuba, Leszek Zdawski, Jurek Dominiak and our colleagues from Germany Zeev once again learned that was not easy to live through the night sober. Next day, yachting on the waters of the Dąbie Lake and around Szczecin put everyone back on their feet. Jarek Wistuba arranged another attraction for Zeev and took him to a classical music concert in Berlin. Having returned to Warsaw, where he was to catch a plane back to Israel, Zeev decided that he liked it here and was reluctant to go home. He promised to meet us again next year. Perhaps even in a larger group and for certain with June Avecilla from Philippines.

Change on the Bridge of the Giżycko Fleet

The annual Convention of the Giżycko Fleet included the ceremony of handing over the insignia.

Tomek Balcerowski – the outgoing commodore, handed the epaulettes over to Darek Dziedziul – commodore for the term of 2017 - 2019. Jacek Stajuda is Vice-Commodore for this term. During the Convention we elected Mciek Matwiejczuk as Rear Commodore. Tomek Grabowski will continue as the Fleet Secretary.

Piotr Pajdowski
RC East Europe

Translation from Polish
Ela Pulawska



Kocham Mazury



Mazury - Gizycko



Hej Mazury

Powrót do źródeł

Zeev Matar – Redaktor Naczelny Rotafloat ponownie pływa z nami w Polsce

Zeev był u nas na Mazurach wraz z Komandorem Międzynarodowym Junem Avencilia w 2016 roku. Pływaliśmy na Mazurach. Jest to kompleks połączonych jezior na północnym wschodzie Polski. Tu jest polskie centrum żeglarstwa śródlądowego. Zeev przyjechał ponownie w maju tego roku, aby uczestniczyć w dorocznych imprezach dwóch Flot: Floty Giżycko i polsko – niemieckiej Floty Pomerania. Zeeva przyciągnęły też do Polski jego rodzinne korzenie i znajomość naszego języka. Pierwszym przystankiem Zeeva była wizyta w Warszawie w Klubie Rotary Warszawa Belweder. Podczas wzruszającego wystąpienia opowiedział o swoich emocjach wobec Polski. W drodze na Mazury Maciek Matwieczuk zawiózł Zeeva do Tykocina. Celem było zwiedzenie słynnej synagogi. Przed regatami w Giżycku, zaprosiłem moją załogę na trening na moim jachcie na jeziorze Beldan. Oprócz Maćka Matwieczuka i Zeeva w załodze wystąpiła Anna Czarczyńska – Prezydent RC Warszawa Belweder. Całą noc pływaliśmy po południowej części Wielkich Jezior Mazurskich. Później, gdy z Zeemem wspominaliśmy cały pobyt w Polsce, uznał te nocne pływanie jako jeden z najbardziej ekscytujących momentów. Jeszcze tamtej nocy Zeev zadzwonił do Juna Avencilia na Filipiny z pytaniem, dlaczego w tę niesamowitą noc nie kołysz się z nami na łódce w środku mazurskiej głuszy. Jun solennie obiecał, że za rok przyjedzie i razem będziemy doznawać uroków nocnego pływania.

Regaty w Giżycku

W tym roku do Hotelu Europa w Giżycku zjechało 18 załóg. W sumie wszystkich uczestników i osób towarzyszących doliczyliśmy się około 100 osób. Dużą i dominującą grupę stanowiły koleżanki i koledzy z Litwy. Z roku na rok powiększa się grupa litewska grupa uczestników regat. Jak zwykle, czwartek wieczór przy ognisku to czas radosnych powitań i zagrzewania do walki na wodzie. Joasia Dzienis – właścicielka hotelu, członkini RC Giżycko, też jak zwykle zadbała o treściwe powitanie załóg. Nad organizacją, również tradycyjnie czuwała Ewelina Jelec. Piątek, pierwszy dzień regat zaczął się od oficjalnego otwarcia z udziałem władz miasta. Następnie wypłynięcie na jezioro. Wszystkie załogi startowały na takich samych łódkach Maxus 24 (7,2 metra długości, łódź kabinowa), dostarczonych przez organizatorów. Pływaliśmy 3 krótkie biegi i jeden długi. Było słońce i był wiatr. Wieczorem zaś Doroczne Zgromadzenie Floty Giżycko. W tym roku nastąpiło przekazanie insygniów i wybór Rear – Commodore (komandora rufowego). Jako gość wieczoru wystąpił znany polski żeglarz regatowy, uczestnik Igrzysk Olimpijskich – Piotr Cichocki. W sobotę, w drugim dniu wyścigów też było słońce, ale wiatr zgasł niedługo po hejnale. Pobujaliśmy się trochę wokół jeziora i trzeba było wracać do portu. Za to wieczór po ogłoszeniu wyników i rozdaniu nagród wypadł szampańsko. Jak zwykle. Wielką niespodzianką tegorocznych wyników było drugie miejsce Darka Dziedziula – z RC Gdańsk- Sopot – Gdynia, obecnego Komandora Floty Giżycko. Zwykle wygrywał regaty, ale tym roku ustąpił załodze Piotr Konstantynowicza. Za rok rewanż.

Po regatach

Za to w niedzielę wiało. Inne załogi rozjechały się do domów. Nasza załoga postanowiła dalej pływać, nadal na łódce z regat. Ania, Zeev, Maciek i ja popłynęliśmy na północ do Sztynortu. Ryba w gospodzie „U córki rybaka” na długo zapadła w naszej pamięci. Niedziela, czwarty dzień spotkania, to był niestety koniec dorocznego święta rotarian żeglarzy w Giżycku.

Zeev jednak został na następne dni. Marek Stawicki i Uwe Schuetze z RC Giżycko zawieźli Zeeva do Wilna, stolicy Litwy. Było to dla niego kolejne spotkanie z niezwykłą polską i żydowską historią. Przed II wojną światową Wilno należało do Polski, a znaczącą część jej mieszkańców stanowiła ludność żydowska.

Z Wilna przez Warszawę Zeev podążył do Szczecina na spotkanie Floty Pomerania. Był to dla niego kolejny przeskok kulturowy. W dobrych rękach Jarka Wistuby, Leszka Zdawskiego, Jurka Dominiaka oraz kolegów z Niemiec Zeev kolejny raz przekonał się, że nie łatwo jest przeżyć na trzeźwo wieczór. Następnego dnia pływanie po wodach jeziora Dąbie i wokół Szczecina przywróciło wszystkim czucie w rękach i nogach. Jarek Wistuba zapewnił Zeevowi kolejną atrakcję i zabrał go na koncert muzyki klasycznej do Berlina. Po powrocie do Warszawy, skąd miał samolot do Izraela, Zeev stwierdził, że u nas jest dobrze i nie chce wracać do domu. Obiecał, że za rok znowu będzie z nami. Może z większą grupą i na pewno z Junem AVECILIA z Filipin.

Zmiana na Mostku Floty Gیزیcko

Podczas Dorocznego Zebrania Floty Gیزیcko nastąpiło przekazanie insygniów.

Tomek Balcerowski – ustępujący komandor, przekazał epolety Darkowi Dziedziulowi – komandorowi kadencji 2017 - 2019. Vicekomandorem tej kadencji jest Jacek Stajuda. Podczas Zebrania wybraliśmy komandora rufowego – Maćka Matwiejczuka. Sekretarzem Floty nadal jest Tomek Grabowski.

Piotr Pajdowski
RC East Europe



**Masurian Lake
Album Pictures**
Please click to open

**AGM Gیزیcko Fleet
Album Pictures**
Please click to open

**Gیزیcko Regatta
Album Pictures**
Please click to open

Credit for Albums Pictures
Piotr Pajdowski
RC East Europe

Editing & Layout

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant



Rotary Club Warszawa Fryderyk Chopin

86389

Rotary International D 2231 Poland

www.rotary.org.pl



Biuletyn klubowy nr 9/2017 (48)

Z dnia 29.05.2017



Nasz klub:

Prezydent:

Halina Rajska

halina_rajska@o2.pl

Sekretarz:

Paweł Rosiński

pawel-rosinski@tlen.pl

Liczba członków: 22

Spotkania:

poniedziałki godz. 18:00

Hotel InterContinental

ul. Emilii Plater 49

Program:

Prezentacje młodzieży z wymiany RYE,
sprawy klubowe

Goście: Teresa Sendor, Edyta Filipowicz,
Iwona Roscoe, Marcin Kotlicki, Raul Sanchez

Zdjęcia: Anna Sendor - Urban, Maciej
Matwiejczuk

Tekst: Iwona Leśniewska, Maciej Matwiejczuk

Opracowanie: Iwona Leśniewska

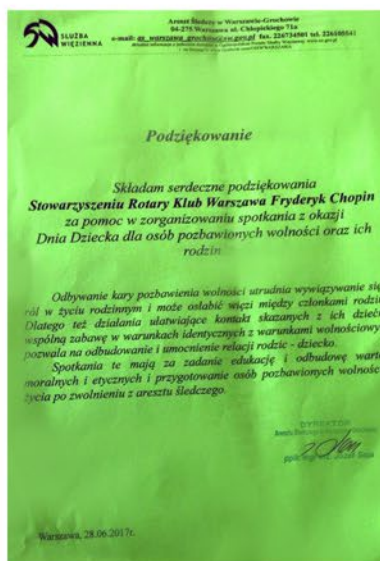
Spotkanie 29.05.2017

Jak co roku, młodzież przebywająca w Polsce w ramach programu RYE, pod koniec rocznego pobytu przygotowała prezentacje przedstawiające ich kraj, rodziny i szkoły. Wszyscy bardzo dobrze poradzili sobie z tym zadaniem, które jednocześnie było sporym wyzwaniem, ponieważ mówili wyłącznie w języku polskim. Prezydent Halina Rajska każdemu z nich wręczyła nagrodę. Będziemy tęsknić za Annouck z Francji, Thais z Brazylii, Zeelynem z USA oraz Ulą i Jessie z Tajwanu. Prezentacjom przysłuchiwali się nasi goście z RC Konstancin: Teresa Sendor, Iwona Roscoe i Marcin Kotlicki. Ich klub tym roku po raz pierwszy będzie brał udział w programie RYE, dlatego chętnie skorzystają z naszych doświadczeń.





Po raz drugi nasz klub był organizatorem Dnia Dziecka dla dzieci osób pozbawionych wolności w więzieniu na Grochowie. W dwie kolejne soboty dzieci osadzonych miały okazję spotkać się ze swoimi rodzicami przy zabawie i słodkim poczęstunku, którego sponsorem był RC Warszawa Fryderyk Chopin oraz Joanna i Joachim Franke. Klub otrzymał podziękowanie od dyrektora więzienia.



Przypominamy o przynoszeniu książek do bookstopów. To wieczne zapotrzebowanie wynika z tego, że znacznie częściej są z bookstopów wyjmowane niż dokładane. Od tej pory wszystkie egzemplarze będą znakowane pieczątką naszego klubu, co jednak zasadniczo nie zmieni pewnie sytuacji, tak więc apel o dostarczanie książek jest nieustający.

W dniach 18-22 maja br. w Giżycku odbyły się XV Regaty o puchar gubernatora dystryktu 2231 i memoriał św. Jacka Nankiewicza (założyciela RC Giżycko). Do walki o puchar stanęło 18 załóg z klubów z Polski, Litwy, Niemiec i Izraela. Walka była zacięta i pasjonująca ale zawsze fair play. Klub RC Warszawa F. Chopin reprezentował Maciej Matwiejczuk. Załoga mieszana w składzie Piotr Pajdowski oraz Anna Czarczyńska z RC Warszawa Belweder, Maciej Matwiejczuk z RC Warszawa F. Chopin oraz Zeev Mataar z RC Hajfa zajęła bardzo dobre 7 miejsce. W trakcie pobytu w Giżycku odbyło się także doroczne zgromadzenie floty IYFR Giżycko. Podsumowano dotychczasowe osiągnięcia i dokonano wyboru nowego mostka floty na lata 2017-2019. Komandorem floty został Dariusz Dziedziul, wice komandorem Jacek Stajuda i drugim wice komandorem Maciej Matwiejczuk. Komandorzy zaprosili wszystkich do udziału w dorocznym spotkaniu floty IYFR Area 1 19-22 października 2017 w Alicante oraz do udziału w starcie regat VOLVO Ocean Race.

Ponadto odbyło się także drugie spotkanie ICC Polska - Litwa. Kluby z Polski i Litwy zacieśniają współpracę, powstały już pomysły wspólnych projektów. Litwini zapraszają serdecznie na doroczną sportową Rotariadę w pierwszą sobotę września. Szczegóły zostaną podane już wkrótce. Przy najbliższej okazji reprezentant RC Warszawa F. Chopin w ICC Polska - Litwa - Maciej Matwiejczuk, przedstawi możliwości współpracy z klubami z Litwy.





Two Lyforian members of the Gizycko Fleet are the silver medalist's of six men team in the Polish ORC Sailing Championships(sea yachts)

HOST CITY



Offshore Racing Congress Polish Sailing Championship 21.7 – 23.7.2017 Gdansk

The Offshore Racing Congress (ORC) is an international body for the sport of competitive sailing and is responsible for the establishment and maintenance of rating and classification standards used to define offshore, that is marine (sea or ocean) as opposed to inland (such as canals, rivers and lakes), yacht racing handicap categories



In Gdańsk on the Baltic Sea side from 21 to 23 July took place Polish Open Sailing Championships. Yachts rang from 8 meters to 18 meters. There were competitors from 14 countries - 52 teams. Foreign crews treated the regattas as preparations for the European Championships, which will begin after the end of the Polish Championship.

The regatta formula assumes a time conversion factor according to a special coefficient derived from the yacht measurement (ORC). The time taken at the finish line was calculated according to the coefficient of each yacht and on that basis players were classified.

Dariusz Dziedziul - Commodore Giżycko Fleet and Piotr Pajdowski – Region Commodore of the Eastern Europe Region were members of the 7th crew captained by Tomek Cichocki, taking on the 18-meter s/y “Baltico” yacht. Short runs and long runs were played on the Gulf of Gdansk. During the first day of the race wind was about 20 knots and it was raining. „Baltico” was the heaviest regatta unit and bravely cared for the conditions and could compete with lightweight racing machines from abroad. Competitors torn the sails – genaker. On one of the yachts broke the mast. The third day was very light wind and the situation turned away. Swedish and German yachts kevlar made, semi-professional crews sailed away just after the start. After converting the results team captain. Cichocki on a Baltico won a silver medal, the title of vice-champion of Poland ORC. For Tomek Cichocki's crew it was also a kind of training before the European Championships. Sunday evening, after prizes were a great feast. From Monday morning, July 24th, the crew was rushed to prepare the yacht before a really serious attempt at team action, before the European ORC Championships. More than 80 yachts, including the few of professional teams, have come forward on extremely fast racing machines. How good it will be, the crew of Tom Cichocki has a chance, because the Baltico is heavy and dignified, but also very stable. We are looking forward and hope for strong wind.

Dariusz Dziedziul
Commodore Giżycko Fleet

Piotr Pajdowski
Region Commodore East Europe



First row from left cpt Tomasz Cichocki, third from left Dariusz Dziedziul,
Second row first from left Piotr Pajdowski



Editing & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

Pictures Album

Polish ORC Sailing Championship

[Please click to enter](#)



[Click to go back to Table of Contents](#)

International Yachting Fellowship of Rotarians



Dedicated poem to our Fellowship in Germany

Iyforians Gedicht

Yachting Fellowship of Rotarians
Wir sind die Könige der Meere
Kreuzen jeden See, jeden Ozean
Mit vom Winde gefüllten Segeln.
Wir sind die vereinten Flotten
Rotarisch verbundener Bürger
Durch die Liebe zum Meer
in jedwedem Land
Segelt weiter, Freunde
Mit offenen Segeln der Freundschaft.
Brüderlich sei unsere Sprache,
Wodka und Champagner
Unsere immer währenden Trophäen.
Rotwein und Rum
Fließen in unseren Venen,
Herausforderungen zu trotzen
Und verhängtem Geschick zu widerstehen
Viva and Aisa
Großartiges Mabuhay all meinen Freunden,
Friede allen Iyforians!
Shalom sei unser Gruß, Amen
Segelt weiter, Matrosen,
Gewinnt uns guldene Pokale,
Denn Seefahrt ist unsere gemeinsame Liebe,
Als begeisterte Segler
Bei rauem Wetter und unter strahlender Sonne.
Segelt weiter
Matrosen und Skipper
Mit grenzenloser Freude.
Es lebe IYFR und dreht weiter das Rad,
Behütet durch Poseidon, dem alten Griechen.
Fordert heraus unsere Schiffe und den Wind
Und lasst uns gewinnen, Gemeinsam.

Zeev

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant Credit Poem translation by Dieter Ambrosius

GERMANY – POLAND-SWEDEN POMERANIA FLEET



SZCZECIN
26th May - 28th May
2017

Last weekend in May 17 2017 - IYFR-Pomerania Fleet



Blue sky, a soft breeze from north-east, we are in the Dambie lake near Szczecin in Poland laying inviting with the boats on it in the sun at 27 degrees Celsius. Only the ropes of the sailing boats are rattling at the mast, sometimes. Best conditions for the opening of the season of Pomerania Fleet, we thought, but the crew was exclusive.

A few enrolled, dates clashed, private and Rotarians reasons overlapped with other IYFR-activities in friendly fleets or with a Rotaract conference. So the opening of season event changed in a sailing turn of international Iyfrans from Poland, Germany and Israel. Nevertheless it was in many ways a remarkable meeting.

Dieter Ambrosius, secretary Fleet, Germany





Letztes Wochenende im May '17 - Pomerania IYFR-Fleet

Blauer Himmel, eine leichte Brise von Nordost, der Dambie See bei Stettin in Polen liegt einladend bereit mit den Booten in der Sonne bei 27 Grad; nur die Falle der Segelboote klappern am Mast, gelegentlich. Optimale Vorraussetzungen für die Saisoneroöffnung der Pomerania Fleet waren es, doch blieb der Kreis der Teilnehmer exclusiv.

Wenige Meldungen nur gingen ein, Termine überschnitten sich, private, rotarische, in den befreundeten IYFR-Flotten und bei den Jungen von Rotaract. So wandelte sich die Saisoneroöffnung in einen Segeltörn international gemischter IYFR-ians aus Polen, Deutschland und Israel. Dennoch war es in vieler Hinsicht ein spektakuläres Meeting.

Dieter Ambrosius, Sekretär Deutschland

Editing & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

Pictures Album
Please click to enter

International Yachting Fellowship of Rotarians



Dedicated poem to our Fellowship in Slovenia

Pesem iyforovcev

Yahting bratovščina Rotarijcev,
smo kralji morja,
prečimo vsako jezero ali ocean.
Z vetrom napolnimo jadra.
Vsi smo združeni v flote
porazdeljene pod različnimi zastavicami
v katerikoli koli državi.
Ljubimo se z vsemi morji.
Pojdite jadrat prijatelji,
z odprtimi jadri prijateljstva.
Bratstvo je naš jezik,
vodka in šampanjec
sta naši večni trofeji,
vino in rum
je naša kri v žilah, da delujemo.
Pozdravi Viva, Aisa in Mabuhay
vsem, ki ste moji prijatelji,
mir za vse vas Iyforijce.
Shalom je naša beseda, Amen.
Nadaljujte plovbo mornarji,
osvajajte za nas jadrane pokale.
Yachting je naša skupna ljubezen,
predani smo mornarji,
v oblačnem vremenu ali v sijočem soncu.
Nadaljujte plovbo,
mornarji in funkcionarji,
naj se naša zabava nikoli ne konča.
Naj živi Yachting bratovščina Rotarijcev
Poženimo naprej naše kolo,
blagoslovljeno od grškega Pozejdona,
da izzovemo naše barke in vetrove
in za vedno zmagamo.

Zeev

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

Credit Poem translation by Marko Murm RC Balkans

Slovenia

Slovenia Fleet



REGATTA CARNIOLA

27/5/2017 – 30/5/2017



9th traditional regatta Carniola 2017

Murter-Kornati-Vodice-Šibenik Solaris

May 27th – 30th, 2017

This year Iyforians, Rotarians and our friends gathered on the island of Murter in Croatia. At Marina Hramina we hired monotype boats Bavaria-46 for 8 crew members. Total 155 participants from 24 Rotary clubs in 8 countries and sailed out on 19 sailing boats and one motor yacht, if we not mention the regatta boat Kon-Tiki with the regatta captain Sebastian Levstik.

Interests in the regatta were again well above expectations this year, so the late entries and those with a requirement of 3-5 vessels were unfortunately rejected. 150 participants is the maximum that can withstand in Dalmatian restaurants and especially the maximum that regatta organizers allow to create a true Rotarian friendly atmosphere. More than half of the participants are returnee acquaintances. Many return each year or every other year. At the end of the regatta participants already confirm booking for the next year. It was a case also this time - already 10 crews registered for 2018 ! So, also you, do hurry up with the applications for the next regatta Carniola ... Link is already opened [Regata Carniola 2018](#)

High level of regatta organization, the beauty of the selected sailing route, excellent cuisine and especially true Rotarian socializing has never disappointed us on the regatta Carniola. Our expectations are higher every year, so we had managed also in this Regatta so perform fantastic event that nevertheless had thrives, finding ourselves positively surprised. That was the case of this year, too.

The first day we set sails from the marina to the southern part of the Kornati islands berthing in the evening at the tavern on islet Ravni Žakan. Already at the pier real Rotarian atmosphere has developed with greetings of old friends who have not met for some time, with presenting of new visitors from Slovenia and abroad. Our old friends come from Germany and Switzerland, but this time there were also novices from Macedonia, Serbia and even Scotland. At the chords of Dalmatian trio we were served with seafood and wine. Loud presentation of crews had promised the fighting spirit at the next day start.

The second day we passed two navigation legs among the Šibenik archipelago. Slight breezes of thermal winds provided pleasant sailing, real coping of tactics on the start and measuring sails trimming skills. The pause between two legs was too short for swim refreshing of crews in still quite fresh Adriatic sea. This evening we were hosted at ACI marina Vodice, from where buses drove us into the hinterland of the coast to the famous Prosciutto House ROCA. Dinner under the olive trees, on stone tables filled with meat baked under a baking lid and accompanied by songs of Dalmatian quartet which visited each table, made this event very special.

On the third day our the first race started in front of the Tribunj at Vodice. Favorable wind enabled the excellent sailing navigation in two legs, with a stopover at buoys in the bay of island Tijat. We finished the day in the touristic resort Solaris at Šibenik, where we had a final dinner in restaurant »Dalmatinsko Selo« in shape of genuine Dalmatian village. After the dinner we moved to the Legend bar on the Pirate ship, were announcement of results took place, followed by price giving ceremony.

Of course, our regatta had the spirit of Rotary charity note. Together with the contributions of participants, sponsors and organizers from RC Ljubljana Carniola over € 12,000 had been collected to help the »Center Vidim cilj« (»I can see the goal«) to organize summer activities for blind youth and to help the para-Olympic swimmer to buy for him adapted vehicle.

The last day, as always, turned out in returning to the main marina, in returning charter boats and in quick farewell to the crews who have already rushed home. But each of us was leaving with a smile on his face and his greeting: "See you next year again!"





9.tradicionalna regata Carniola 2017
Murter-Kornati-Vodice-Solaris Šibenik
27.-30.maj 2017

Letošnje leto smo se lyforovci, rotarijci in naši prijatelji jadrnanja zbrali na otoku Murter na Hrvaškem. V marini Hramina smo najeli monotip jadrnice Bavaria-46 za 8 članov posadke. Skupaj 155 udeležencev iz 24 Rotary klubov in 8 držav je zaplulo na 19 jadrnicah in eni motorni jahti, da o regatnem odboru Kon-Tiki s kapitanom regate Sebastianom Levstikom niti ne govorimo.

Interes za regato je bilo tudi letošnje leto nad pričakovanji, tako da so bile pozne prijave in tiste z zahtevo po 3-5 plovilih žal zavrnjene. 150 udeležencev je tisti maksimum, ki ga še prenesejo dalmatinske restavracije in predvsem maksimum, ki organizatorju regate še omogoča ustvariti pristno rotarijsko prijateljsko štimungo. Več kot polovica udeležencev je namreč starih znancev. Mnogi se vračajo vsako leto ali vsako drugo leto. Ob koncu regate že potrjujejo rezervacijo za naslednjo regato. Tako je že danes prijavljenih 10 posadk za leto 2018 ! Pohitite s prijavo tudi vi... Link je že odprt [Regata Carniola 2018](#)

Visok nivo organizacije regate, lepote izbranih jadrlnih rut, odlična kulinarika in predvsem tisto pravo rotarijsko druženje nas na regati Carniola še nikoli ni razočaralo. Naša pričakovanja so vsako leto višja, a organizatorjem kljub temu uspeva, da nas še vedno pozitivno presenetijo. Tako je bilo tudi letos.

Prvi dan smo zapluli iz marine do južnega dela Kornatov in se zvečer vezali pri gostišču na Ravnem Žakanu. Že na pomolu se je razvilo pravo rotarijsko vzdušje: pozdravi ob srečanju starih prijateljev, ki se že nekaj časa niso srečali, predstavljanje novih obiskovalcev iz Slovenije in tujine. Naši stari znanci prihajajo iz Nemčije in Švice, a tokrat so prišli tudi novinci iz Makedonije, Srbije in celo Škotske. Ob akordih dalmatinskega tria so nam postregli z morskimi dobrotami, ki smo jih zalivali z vinom. Bučna predstavitev posadk je obetala borbeni duh na štartu naslednjega dne.

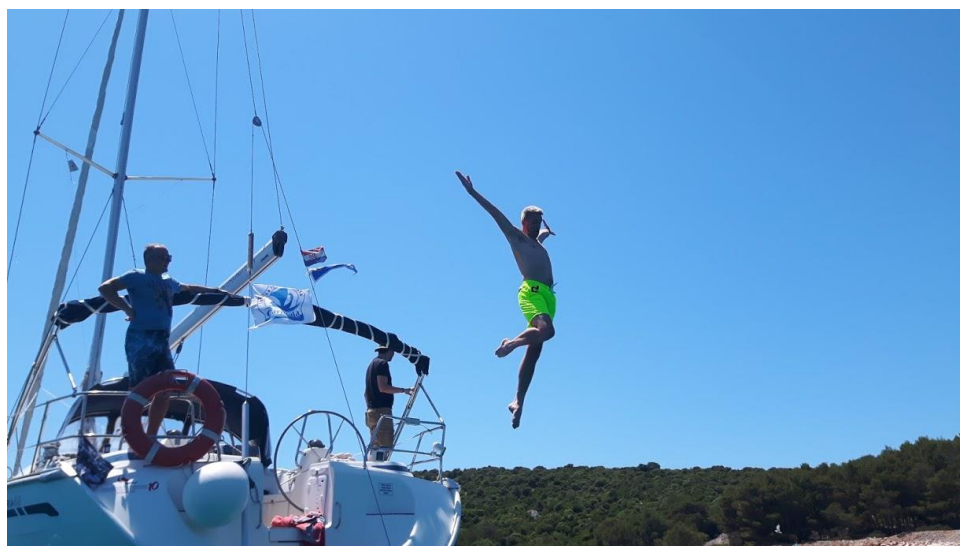
Drugi dan smo opravili dva tekmovalna navigacijska plova po Šibeniškem arhipelagu. Rahle sape termičnih vetrov so zagotovile prijetno jadrnanje in pravo kosanje jadrcev v taktiki štartov in merjenju znanja pri trimanju jader. Postanek med plovoma je bil kar prekratek za osvežilno kopanje posadk v sicer še dokaj svežem Jadranu. Tokrat smo bili gosti ACI marine Vodice, od koder nas je organizator z avtobusi odpeljal v zaledje obale do znane pršutarne Kuća pršuta

ROCA. Večerja pod oljkami, na kamnitih mizah, ob pečenju izpod peke in spremljavi dalmatinskega kvarteta, ki nam je ob mizah pel zaželeno skladbo, je bila res nekaj posebnega.

Tretji dan smo štartali prvi plov kar pred Tribunjem pri Vodica. Ugoden veter je omogočil odlično jadranje v dveh navigacijskih etapah z vmesnim postankom na bojah v zalivu otočka Tijat. Dan smo zaključili v turističnem naselju Solaris pri Šibeniku, kjer smo imeli zaključno večerjo v Dalmatinskem selu, restavraciji v formi prave dalmatinske vasi. Po večerji smo se preselili v Legend bar na Piratski ladji, kjer je sledila razglasitev rezultatov in svečana podelitev pokalov.

Seveda je imela regata tudi v duhu rotarijstva svojo dobrodelno noto. Skupaj s prispevki udeležencev, sponzorji in organizatorjem RC Ljubljana Carniola smo zbrali preko 12.000€, ki bodo pomagali centru Vidim cilj organizirati poletne aktivnosti za slepo mladino in para-olimpijskemu plavalcu kupiti zanj prilagojeno vozilo.

Zadnji dan je kot vedno pomenil vračanja v matično marino, vračanje čarter bark in hitro slovo posadk, ki so že hitele proti domu. A vsak je odhajal z nasmehom na obrazu in pozdravom: »Naslednje leto se vidimo spet !«





Pictures Gallery



[Click photo to view Gallery](#)

Article

Marko Murn

Regional Commodore for IYFR Balkans

Editing & Layout

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant



Slovenia Fleet



Sailing for tomorrow - Jadranje za jutri 20th -27th May 2017



Blind Crew



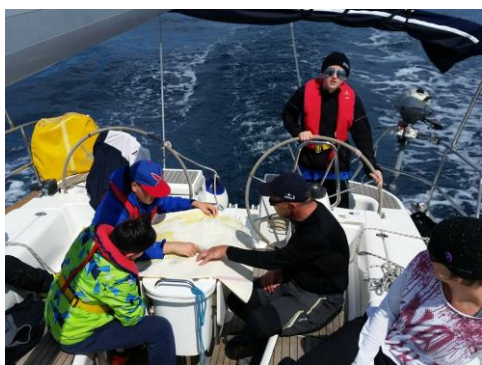
Blind Crew no 2

From May 20th to 27th 2017 eight Slovenian educational institutions sent their pupils to a three days sailing program for youth with special needs or for youth from social endangered families. All together 48 pupils and their 16 companions have experienced the joy of sailing, teamwork on board and fun on the sea. Rotarians under the coordination of Slovenian IYFR fleet have helped to make it happen. We triggered the resources within three Slovenian Rotary clubs (RC Škofja Loka, RC Medvode in TC Ljubljana Šiška).

Slovenian district 1912 and IYFR Area-1 to collected more than 6000€. As we are just receiving back the reports from the participants, we herewith publish one of the school reports, written by the professor participant. It tells you everything: *Use PDR or Word file attached*



Crew of the younger



Navigation lesson



Rowing dinghy



Helping each other



Neptune baptism



First paddles on SUP



Swim in a group



Article
Marko Murn
Regional Commodore for IYFR Balkans

Editing & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant





REPORT from SAILING FOR TOMORROW

From May 20th to 23rd 2017, the pupils of primary school Jela Janežiča have for the fifth time in a row participated in the project »Sailing for tomorrow« in organization of ŠD »Sappa« Kamnik. As in earlier years also this year's project and its organizers served with an unforgettable experience for all stakeholders. For the first time in the form of a circular journey through Kvarner. Special thanks go to the donators from Rotary Clubs and IYFR.

"It is very suitable for teenagers who are in a period when they feel a lack of decision-making power, have a problem with authority and respect thereof."

Sailing is and remains a great motivation and a strong educational approach in a period when we are preparing for it and therapeutic agent during its implementation. Healing effect on troubled students, those who have problems in communication and interaction with others, who are short-tempered and react in an affect or who have

difficulty in team collaboration. Very suitable for teenagers who are in a period when they feel a lack of decision-making power, have a problem with authority and respect thereof. Many such children are labeled as educational problems and therefore difficult to be awarded, in our case, this is sailing.

Effects

...

Even after two weeks on solid ground, the effects of the elements of water activity »Sailing for tomorrow« are still visible. A group of pupils who shared the experience communicate with each other more, are less short-tempered and less frequent visit counselor. Despite the approaching end of the school year, it did not detect the normal voltage, and it is less pronounced. They still share their fresh pleasant memories. Teachers are mentioning more conciliatory and less interpersonal conflicts among students and as well with teachers.

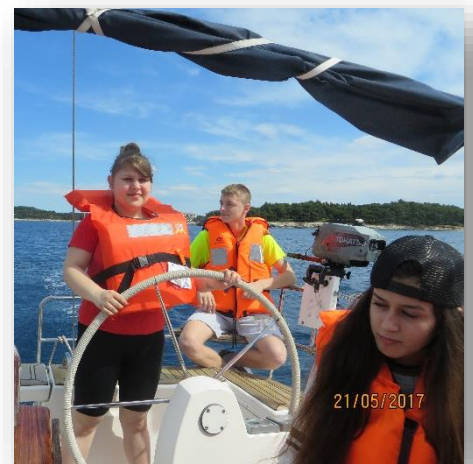


Profile of participants

This year the event attended 6 our students: 2 students from a special educational program in age of 18 years and 4 girls from a lower educational standards program in age of 13 to 15 years. Some students come from financially weak families and families with several children. Until now they have not had experience with holidays at sea, while their discovering Slovenia has been limited only to the opportunities offered by school. Some students as eldest in the family are forced to help mothers in home supply. They do not have much social contacts with peers in their environment

because they establish them hard, due to their deteriorating cognitive abilities and clumsiness in communication. So far they have not had opportunities to participate in a group outside the school environment. The group of girls is very heterogeneous. Some of them come from a bilingual environment or from a completely different cultural and religious background. All in adolescence, with all the emotional and behavioral patterns. It is difficult to subordinate authority, they are very affective in their responses and prone to excessive dramatization. Often they seek the support of the advisory

service in the form of conversations. They are coming in conflict situations among them that they can not solve by proper communication. Due to personal concerns, direct alive communication is rare, they do prefer to cultivate contacts through online media, which is not good for healthy social development.



This year's version of the project SAILING FOR TOMORROW was different from previous ones, when we were in the evenings returning to the same marina. This year we have made a big circle and never slept in the same place. Anchorage in the bay Maračol on island Unije was unique. Evening and morning scenery conjured the scene for a postcard. We had not missed departure on the toilet and the greetings from German tourists on other boats moored in marina. Fresh air and calm sea bay had a therapeutic effect for morning annoyance or for indignation upon awaking and standing up. Also the berth in town Cres on Cres island well served us by its charms. Pancakes party with its scent conjured up summer evening. Being in a urban area satisfied some to fulfill their desire of modern addiction for shopping. The old town with its scenery surrounded by mountains gives special charm and Mediterranean flair.

Our schedule has been carefully composed. We sailed from Pula around the Cape Kamenjak, visited Cres and Osor, sailed around island Unije, saw dolphins, experienced wind in sails, swam, paddled SUPs, did rowing on dinghy, saw places from perspectives that you can not see from land ... not to talk about things that were not on the program: to experience the depth of the sea, when you can not see the bottom, to overcome the distance without engine, test balance, live in a space of studio and still not feel cramped, to see how we can benefit from small space, to discover the secret corners of the boat, to feed stuff, food, tools, as if we were part of the working team, and most important, that we can breathe, function and live without the Internet :)

written by
profesor
Damjan
Kojzek



Our great thanks to organizers and donators

Finally, thanks to the organizers and donors. They brought up another great and memorable event with long-term effect. Energy, which was shared among the skippers, was well transferred on the participants. Their respectful attitude, kept to each other, was good example for young people and us in the education of millenium generations, who are influenced by different media information that are difficult to process and which supply them with a distorted view of social life and relationships in it. Special thanks to those Rotarians who have enabled the project. It is not only financial assistance, but the wilnessl and the understanding that they are ready to give part of their time and thoughts in preparation work to those who need special approaches in education and growing up. It is an unforgettable experience and the opportunity to influence the development of the individual. Even experienced teachers can not create such experiences in the school environment to be acquired and learned from them and here is the contribution of our loyal sponsors invaluable. With your positive energy,that you share, you fill the life of students and give a big part to the mosaic we are trying to build with Youth. We can say thank you, as we, pupils and attendants, feel it like the wind in the sails that are pulling us in the desired direction.



International Yachting Fellowship of Rotarians



Dedicated poem to our Fellowship in Spain

Poema de Iyforians

Compañeros, navegantes Rotarios
Somos los reyes de los mares
Cruzamos lagos y oceanos
Con las velas impulsadas por el viento
Somos las flotillas unidas
Difundiendo nuestro credo, bajo diferentes Banderas
En cualquier País
Participando en el amor por todos los mares
Seguiremos navegando amigos
Con las velas desplegadas de amistad
La Hermandad es nuestro idioma
Vodka y Champaign
Son nuestros trofeos eternos
Vino y ron
Circulan por nuestras venas
“Viva y Aisa”
“Mabuhay” a todos nuestros amigos
Paz a todos vosotros, marineros Iyforians
Shalom es nuestra consigna, Amén
Sigamos navegando marineros
Ganemos copas y trofeos de viento y mar
Navegar es nuestro amor común
Somos devotos marineros
Con mar tempestuoso o iluminado por el sol
Seguiremos navegando
Marineros y oficiales
Tengamos placer infinito
Viva, la Yachting Fellowship de Rotarios
Pilotemos el timón, atentos a nuestra ruta
Bendecidos por Poseidón, el griego
Desafiando con nuestras naves, los vientos y las tempestades
En el mar de la Vida. Siempre venceremos

Zeev

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

Credit Poem translation to PRC Bernardo Rabassa Asenjo



International Yachting Fellowship of Rotarians



Dedicated poem to our Fellowship in Turkey

Iyforyen Şiiri

Rotaryenlerin Yatçılık Birliği
Biz denizlerin krallarıyız
Her göl veya okyanus geçişimizde
Rüzgarla esen yelkenlerle
Biz birbirimize bağlı filolarız
Farklı flamalar altına yayılmış
Her tür ülkeden
Bütün denizlerle aşk yaşarken
Yelken açmaya devam edin dostlarım
Dostluğa açılan yelkenlerle
Kardeşliğimiz bizim lisanımız
Vodka ve Şampanya
Bizim ebedi ödülllerimiz
Şarap ve Rom
Damarlarımızda akan kanımız
“Viva” ve “Aisa”
Bütün arkadaşlarıma “Mabuhay”
Bütün Iyforyanlara barış diliyorum
Shalom bizim kelimemiz, Amin
Seyir etmeye devam edin denizciler
Bizim için yelken yarışlarını kazanın
Yatçılık bizim ortak aşkımız
Biz adanmış yelkencileriz
Bulutlu havalarda veya güneşin parlağında
Seyir etmeye devam edin
Denizciler ve subaylar
Beraber sonsuz bir eğlence yaşayalım
Çok Yaşa Rotaryenlerin Yatçılık Birliği
Çarkı ilerletelim
Yunan Poseidon tarafından kutsanalım
Yatlarımızı rüzgarla yarıştıralım
Ve hep kendimizi kazandılarım

Zeev

Zeev Matar

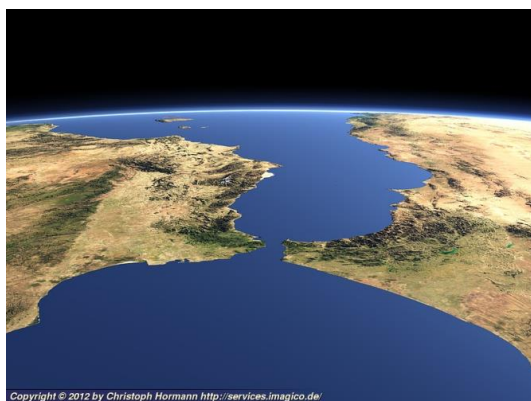


Rotafloat Magazine Editor / IC Special Assistant

Credit for English Poem translation by Ceyda Tuglu



Gibraltar Strait



The legends mention that Hercules used his superhuman strength to push the mountain so to connect the Atlantic Ocean to the Mediterranean Sea forming the Strait of Gibraltar

The strait of Gibraltar known in old ages as Fretum Herculeum in Latin is the channel which connects the Mediterranean Sea with the Atlantic Ocean positioned geographically in the southern of Spain and in the northwestern of Africa.

The Gibraltar strait is bordered by the continents of Europe and Africa, and the counties of Morocco, Spain and the British colony of Gibraltar and the Spanish exclave of Ceuta.

Given names to the strait

The strait was called during the ancient Greek times called "Pillars of Hercules" while the Romans had called the strait as "Fretum Gatitanum " (strait of Cadiz) and in the Middle ages the Arabs had called it " The passage" – Al-Zuqaq

Gibraltar strait data

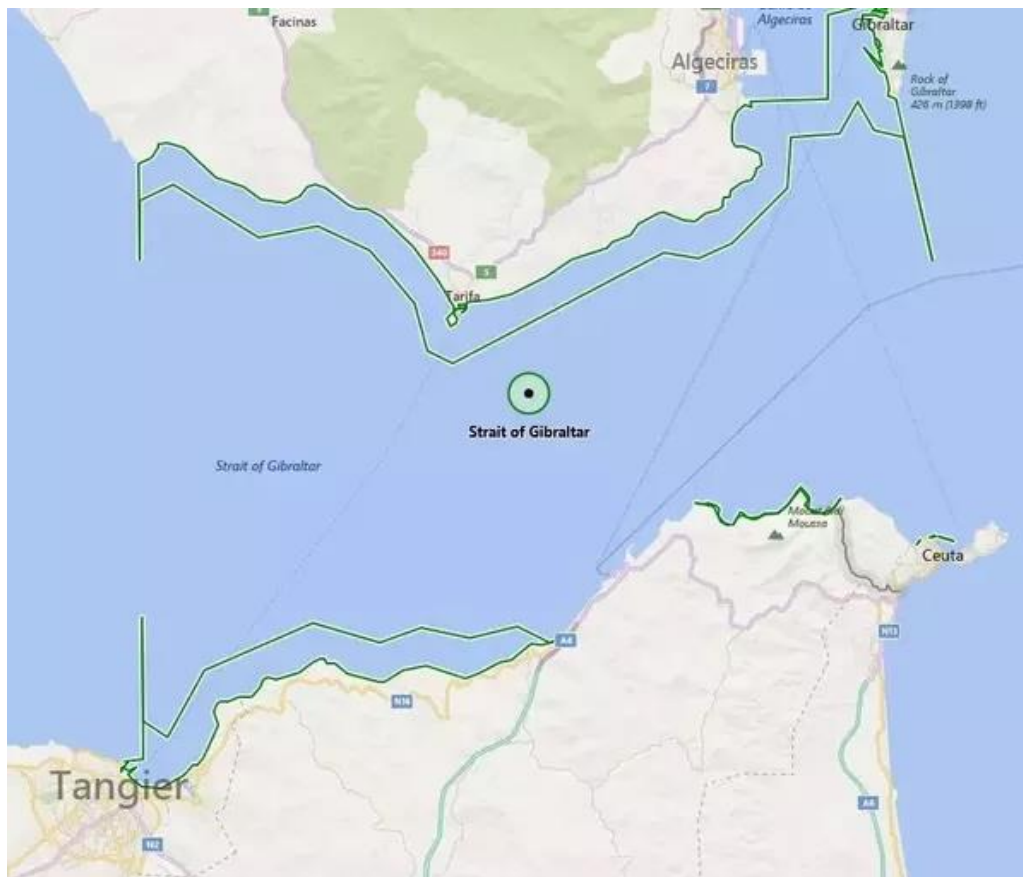
The strait length is 58 km (36 miles) having the width of 13 km (8 miles) between Point Marroqui in Spain and Point Cires in Morocco. The eastern extreme width is 23 km (14 miles) between the Pillars of Heracles known as the Rock of Gibraltar to the north and one of two peaks to the south: Mount Hacho held by Spain near the city of Ceuta.

The western extreme is 43 km (27 miles) widest between the capes of Trafalgar (north) and Cap Spartel (south).

The Strait's depth is between 300-900 meters (160-490 feet) with average depth of 365 meters (1200 feet).



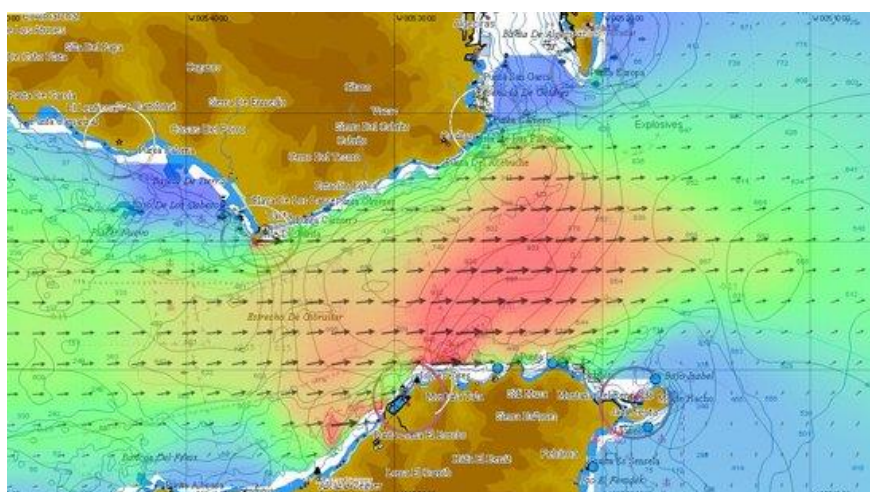
The land and part of sea water controlled by each of the countries in the Gibraltar strait



The currents

The Currents through the Strait of Gibraltar are mainly caused by an exchange of water of different salinity (saltiness) between the Atlantic Ocean and Mediterranean Sea

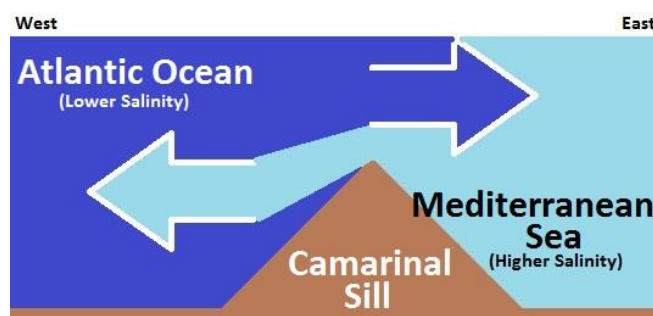
The Currents through the Strait of Gibraltar are mainly caused by an exchange of water of different salinity (saltiness) between the Atlantic Ocean and Mediterranean Sea. A smaller amount of deeper saltier or denser waters do their way from the Mediterranean Sea as outflow while a larger amount of surface waters with lower salinity and density do their way east to the Mediterranean Sea as inflow. Generally, the water flow is doing its way to the east entrance of the Mediterranean Sea also due to an evaporation higher rate.



The shallow Camarinal Sill of the Gibraltar strait which forms the shallowest point is the separating threshold between the Atlantic Ocean and the Mediterranean Sea between the Iberian Peninsula and Africa .

The Camarinal Sill acts to limit mixing water between the cold and less saline Atlantic Sea water and the warm Mediterranean waters.

The Camarinal Sill located at the far western end of the strait near the Espartel Sill at the coordinates [35°56'N 5°45'W](#) [35.933°N 5.750°W](#) at an elevation of -280 m.



NOTA: Las profundidades están referidas al nivel actual del mar

The Mediterranean water is by far more salty than the Atlantic waters that they sink below the constantly incoming water and form a highly saline layer of bottom water. This layer of bottom-water constantly finds its way out into the Atlantic Ocean due to the outflow of the Mediterranean Sea. On the Atlantic side of the strait, a density boundary separates the Mediterranean outflow waters from the rest at about 100 m (330 feet) depth. The Mediterranean outflow water layer can be traced for thousands of kilometers west from the strait, before completely dissolving losing their identity as salted water.

Internal waves

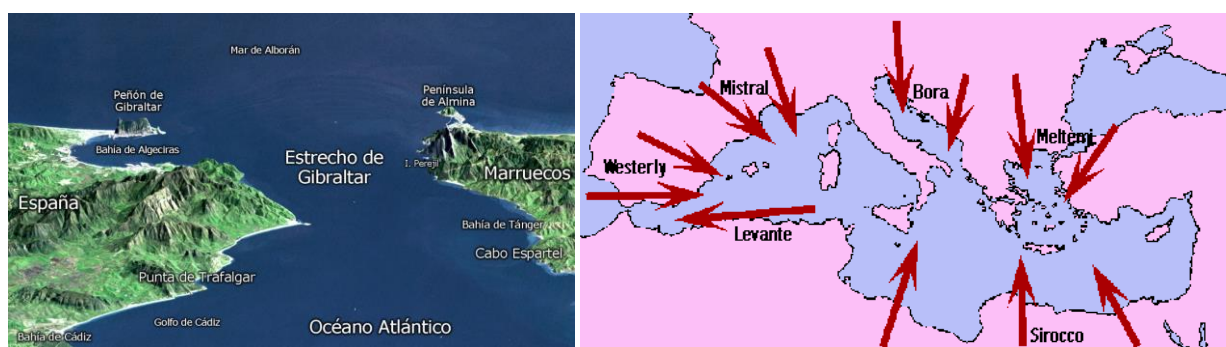
Internal waves (waves at the density boundary layer) are often produced by the strait. Like traffic merging on a highway, the water flow is constricted in both directions because it must pass over the Camarinal Sill. When large tidal flows enter the Strait and the high tide relaxes, internal waves are generated at the Camarinal Sill and proceed eastwards. Even though the waves may occur down to great depths, occasionally the waves are almost imperceptible at the surface, at other times they can be seen clearly in satellite imagery. These *internal waves* continue to flow eastward and to refract around coastal features. They can sometimes be traced for as much as 100 km (62 mi), and sometimes create interference patterns with

The winds

The winds in the strait are mainly easterly or westerly. Cold air masses invade the western Mediterranean from the north, with often stream through as a low-level, high speed easterly wind known locally as Levante also spelled levante.

The Levante is an easterly wind through the Straits of Gibraltar and between Spain and Morocco. It is usually a light or moderate wind bringing with it excessive moisture, heavy dew, local cloud, and thick muggy atmosphere and sometimes rain. It may occur at any time of the year, but is most frequent from July to October and in March and seldom blows with gale force.

Strong winds of western Mediterranean Sea together with damp and rainy weather common in spring causes foggy weather on the Spanish coast for up to two days at a time. There is also a significant exchange of water through the strait. On the surface the currents flows eastward through the centre of the channel, except when affected by easterly winds. This surface movement exceeds a westward flow of heavier, colder, and more saline water, which takes place below a depth of about 120 meters (400 feet). Thus, only the existence of the strait prevents the Mediterranean from becoming a shrinking salt lake.



The Gibraltar Strait legend - Pillars of Hercules

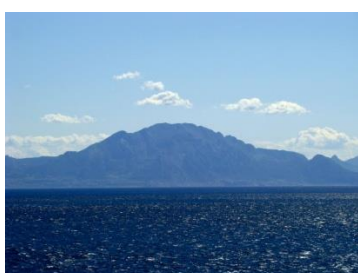
According to the legend the Gibraltar strait was part of the mythological doings of Heracles the Greek, as the Greek mythology is full of tales about gods and goddesses, heroes and villains.

Among them had lived the most popular hero who had ever lived, Hercules, born to the king of gods Zeus and a mortal woman named Alcmena. Hercules who had been half god, half human, a demigod had a super strength, but had to suffer on a scale no human had ever known before. Tormented by a horrible guilt, he had been driven to take on twelve impossible challenges in a quest for redemption, known as "The Twelve Labors of Hercules".

For his tenth labor, Hercules went all the way to the Spanish Peninsula, where he had to bring back the cattle of King Geryon, a three-headed monster. Destroying the Geryon to capture his cattle was half the challenge. The other half was getting there. To reach King Geryon's cattle, Hercules had to venture beyond the Mediterranean Sea into the Atlantic Ocean, but one massive obstacle stood in his way: the mountain range that joined the continent of Africa and Europe and which sealed off the Mediterranean Sea from the Atlantic Ocean.

Hercules decided not to go around the mountain; he instead went through it. He split the mountain into two with one blow from his sword, passed through the narrow strait, found Geryon's cattle and brought it back.

This part of myth was created to explain how the Atlantic and the Mediterranean were joined. The cliffs on each side are forever linked to Hercules and were known by the Ancient Greeks as the Pillars of Hercules. The Northern pillar falls in the continent of Europe and is known as the Rock of Gibraltar. No one knew, then, what was beyond them. To the Ancient Greeks, it was not only the gateway to the unknown; it was a portal between reality and myth. Having gone and come back only increased the reputation of Hercules.



The African Pillar Jebel Musa



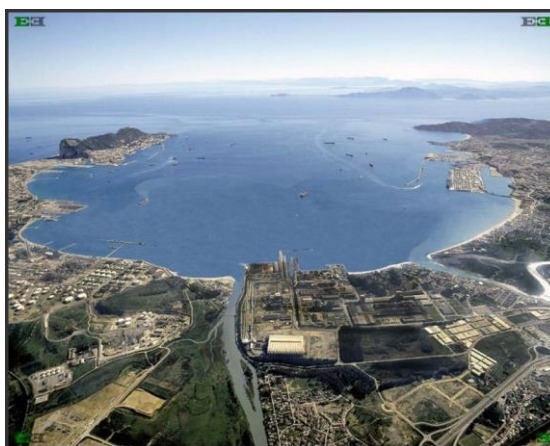
Hercules



The European Pillar-Rock Gibraltar



The Rock of Gibraltar is a very large monolithic limestone promontory



Jabal Tariq was the Moorish/Arabic name of the Rock of Gibraltar and means the Rock of Tariq



© robepco - Fotolia.com

#13248819

Ancient poetry of Spain Count Arnaldos

Who was ever sped by fortune
O'er the ocean's waters, say,
As the happy Count Arnaldos,
On the morn of Juan's day?
In his hand he held va falcon,
And he went to chase the game,
When a gay and splendid galley
To the shore advancing came.
All its fluttering sails were silken,
All its shrouds of flounces clear,
And the gay and clear-voiced helmsman
Sang a song so sweet to hear,
That the waves were calm and silent,
And the noisy storm-wind hush'd,
And the fish that live the deepest
To the water's surface rush'd;
While the restless birds were gather'd,
Listening on the masts, and still.
"O, my galley! – O my galley!
God preserve us now from ill,
O'er the waters of the ocean,
O'er the dark world's troubles far,
O'er the plains of Almeria,
And the straits of Gibraltar.



Sailing in Gibraltar Strait



Yacht sailing crossing the Gibraltar Striate



The Barcelona World Race course is from Barcelona to Barcelona, putting the capes of Good Hope, Leeuwin and Horn to port and the Antarctic to starboard.



The regatta is non-stop race round the world from west to east, with the boats sailing a theoretical route some 23,000 miles in length over approximately 3 months. This theoretical route is calculated using the Great Circle line or orthodromic distance, which is the shortest route traced around Earth on a map. However, in reality the skippers notch up much longer distances and in many cases ten per cent longer, with strategy based on winds and conditions rather than distance on a map.

Following in the tradition of previous editions of the regatta, the starting line was opposite the W Barcelona Hotel (Spain). The skippers made their way down to the Strait of Gibraltar and once through entered Atlantic waters and set course south. The boats moved down past the Equator to rounded the Cape of Good Hope and then into the Indian Ocean. The competing teams passed Antarctica to put the legendary capes of Leeuwin (Australia) and Horn (Chile) to port before returning to the Atlantic and climbing back up north to the Gibraltar Strait and on to the finishing line at Barcelona.

In the first and second editions of the regatta the participants were instructed to pass through Cook Strait in New Zealand. For this third edition, however, the passage through Cook Strait is not mandatory, with the fleet putting the archipelago of New Zealand to the north.

An important element of the race course are the ice limits. The Race Directors set coordinate limits for the boats at certain points, due to the threat of floating ice in the South Indian and Pacific Oceans. It was called the Antarctic Exclusion Zone. (See article on the danger of ice and the round table discussion held in Barcelona in January 2013).

12 climate zones in three months

The Barcelona World Race route takes the boats through almost all of the world's macroclimatic zones and also throws in the difficult tactical sailing in the Mediterranean and through the Gibraltar Strait.

The course can be divided up into 12 clearly differentiated climate zones. These are:

Barcelona – Gibraltar Strait

This Mediterranean leg is some 530 nautical miles (983 km) in length. Weather predictions for this area are much more difficult to make than for the open seas, due to the physical geography of the coast and its proximity, which produce variable and shifty patterns, just miles apart. This is a very tactically challenging leg, particularly during the winter months, where thermal winds are scarce and areas of calm are in abundance.



Through the Gibraltar Strait

Depending on the wind, this can be one of the most tricky points in the race. If there is a storm from the west after the start, or a storm from the east upon return to Barcelona, the IMOCA Open 60s may face a tough test. Strong counter-currents can make for a difficult passage through if the winds are low.

Gibraltar to the Canary Islands

This leg is some 640 miles (1,222 km) long. The boats sail in search of the NE trade winds here.

Through the Canary Islands

The passage through the Canary Islands is a tricky tactical challenge, as the breeze is channelled with considerable variations in intensity. The leeward passage of the islands must also be undertaken with care, due to the numerous windless pockets lurking in the zone, caused by the physical geography of the islands.

The Canary Islands to the Equator

The boats may cover from 1,700 to 1,900 miles during this leg, depending on how they choose to cross the lulls at the Equator. During the initial part of the leg, the entries are pushed by the trades, but these diminish as the boats move closer to the Equator line. The tactics called by the skippers will be governed by the stability of these NE winds.

Crossing the Equator

This is one of the most challenging zones in the race. Here the boats must pass the 'doldrums': a ring of lulls around the planet, of varying breadth according to location and the time of year. This windless area can be as broad as 300 miles wide. During the second half of January the best 'corridor' to get through the doldrums is usually at around 30° W. The boats can take up to four days to get through the equatorial region. Once they're out of it they can hunt for breeze from the numerous squalls nearby.

The Equator to the Cape of Good Hope

In their descent of the South Atlantic, the boats will cover between 3,200 and 3,600 miles. Here everything will hinge on the reign of the SE trades blowing in the Southern Hemisphere and on the evolution of the St Helena anticyclone. As the boats move to lower latitudes and closer to 40°S they will begin to grapple with the 'Roaring Forties'; very strong winds from the west that throw up some huge swell.

Crossing the South Indian Ocean

From the Cape of Good Hope to South Tasmania, the skippers will cross the most difficult stretch in the entire race. These 5,000 miles will be brimming with squalls from the south, and cold and wet conditions from the Roaring Forties. The tactics here will consist of positioning the boat at the best possible angle to take on the squalls generated by these winds.

South Pacific to Cape Horn

A 4,000 -mile crossing where the skippers will yet again hit the Roaring Forties. The approach to the much-revered Cape Horn may dish up thick, dense banks of fog for the boats. Here the handling of the squalls is key as the boats make their obligatory rounding of the Horn. Conditions at Cape Horn, in the Chilean Patagonia, can be gruelling if the winds blow from the NW, sped up by the Andes.

Cape Horn to the Equator

The climb up the South Atlantic stretches over some 3,900 miles. The first tactical dilemma is where to pass the Falkland Islands. The second is how to take on the St Helena anticyclone, often with strong headwinds for the fleet. Here the entries tend to hug the coast of Brazil. The Doldrums will throw up the same frustrations as they did on the way down.

The Equator to the Gibraltar Strait

A leg of approximately 2,550 miles where skippers have to grapple with the anticyclone at the Azores. In this leg, tactical calls are crucial, depending on the distances between the entries.

The passage through the Gibraltar Strait and the final stretch to Barcelona

Here conditions will be similar to those at the start of the race.





The all-women's crew Team SCA have led the other six male crewed boats through the Straits of Gibraltar - the first major milestone of the Volvo Ocean Race.

A daring split with the fleet paid a big dividend for the women's crew and they picked up a handy lead of around 5nm, sailing in a 12kt breeze which looks set to hold through the Straits which mark the entrance to the Mediterranean.

At the 0340hrs UTC report, Team Vestas Wind has chopped a mile out of the Team SCA lead, who were making about 3kts average speed, about half that of the rest of the fleet. But GPS data showed that the women's crews were close to crossing ahead the six men's teams.



October, 2014. Leg 1 onboard Team SCA. Sally Barkow helps lift the sail up on to the stack shortly after the start of the 2014-2015 Volvo Ocean Race on board Team SCA. © Corinna Halloran / Team SCA

Volvo Ocean Race reported it this way in their 0730hrs report:

So how did this happen? At 2050 UTC, Team SCA was one of the first five boats, sailing in the middle of the fleet. That's when they chose to tack north and head closer to the coast to avoid the strong currents in the middle of the Strait.



Article & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant



International Yachting Fellowship of Rotarians



Dedicated poem to our Fellowship in Argentina, Chile, Colombia, Ecuador, Peru, Uruguay

Poema de Iyforians

Compañeros, navegantes Rotarios
Somos los reyes de los mares
Cruzamos lagos y oceanos
Con las velas impulsadas por el viento
Somos las flotillas unidas
Difundiendo nuestro credo, bajo diferentes Banderas
En cualquier País
Participando en el amor por todos los mares
Seguiremos navegando amigos
Con las velas desplegadas de amistad
La Hermandad es nuestro idioma
Vodka y Champaign
Son nuestros trofeos eternos
Vino y ron
Circulan por nuestras venas
"Viva y Aisa"
"Mabuhay" a todos nuestros amigos
Paz a todos vosotros, marineros Iyforians
Shalom es nuestra consigna, Amén
Sigamos navegando marineros
Ganemos copas y trofeos de viento y mar
Navegar es nuestro amor común
Somos devotos marineros
Con mar tempestuoso o iluminado por el sol
Seguiremos navegando
Marineros y oficiales
Tengamos placer infinito
Viva, la Yachting Fellowship de Rotarios
Pilotemos el timón, atentos a nuestra ruta
Bendecidos por Poseidón, el griego
Desafiando con nuestras naves, los vientos y las tempestades
En el mar de la Vida. Siempre venceremos

Zeev

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

Credit Poem translation to PRC Bernardo Rabassa Asenjo



ARGENTINA

Rio de la Plata Fleet



On June 4th the Rio de la Plata Fleet held a meeting of camaraderie to honor his founder Guillermo Arteta, the next International Commodore of the IYFR, which he will take charge during the Atlanta Convention.

The Delta Fleet was invited, which was headed by its Commodore Miriam Caudullo along with several members of the same fleet.

We had planned navigation and a later lunch, but the inclemency of the weather made us perform only lunch at the yacht club San Martín on the riverside of San Fernando, with the attendance of 22 enthusiastic Iyforians who enjoyed such a good moment.

Guillermo was very happy and excited by the participation of so many friends and the affection he received from everyone present.

We wish him the greatest success and we will all be with Guillermo to accompany and support him. Congratulations future International Commodore!

Graciela Marchesano
FC Rio de la Plata fleet





El cuatro de junio la Flota Río de la Plata realizó una reunión de camaradería para agasajar a su fundador Guillermo Arteta, próximo Comodoro Internacional de la IYFR, cargo que asumirá durante la Convención de Atlanta.

Fue invitada la Flota Delta, que concurrió encabezada por su Comodoro Miriam Caudullo junto a varios miembros de la misma flota.

Habíamos planeado una navegación y posterior almuerzo, pero las inclemencias climáticas hicieron que realizáramos solo el almuerzo en el club náutico San Martín en la ribera de San Fernando, contando con la presencia de 22 rotanautas entusiastas que disfrutamos tan grato momento.

Guillermo estuvo muy feliz y emocionado por la participación de tantos amigos y el afecto que recibió de todos los presentes.

Le deseamos el mayor de los éxitos en la nueva responsabilidad que asumirá y estaremos todos para acompañarlo y apoyarlo.

Felicidades futuro Comodoro Internacional!!!!

Graciela Marchesano
Comodoro Flota Río de la Plata

Editing & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant



CHILE

Puerto Varas Fleet



Puerto Varas Sea Scouts



El Heraldo Austral

PANORAMAS

LOCAL

Flota IYFR Chile Puerto Varas recibe premio en el AGM de Punta del Este 2017

Publicado por Paola Alvarez en Abril 6, 2017



La Flota Chile Puerto Varas perteneciente a la International Yachting Fellowship of Rotarians (AGM), recibió el premio, en manos de su Comodoro Hernán Correa V., como la flota con más participantes en el Annual General Meeting de Flotas de Sudamérica.

La delegación Chilena estuvo representada con más de 10 miembros de sus escuadrones de Valparaíso, Santiago y su flota madre Puerto Varas.



En esa oportunidad las flotas de Argentina, Uruguay, Brasil, Perú, Ecuador y Chile, dieron a conocer sus actividades de servicio, entre las que se destacaron las de Chile relacionadas con el programa Scouts Marinos, el cuidado al medio ambiente y un novedoso programa de enseñanza de Bodyboard a niños con discapacidad.

Al importante evento concurrió el Comodoro Internacional de IYFR, Jun Avecilla de Filipinas, Guillermo Arteta, Vice Comodoro Internacional y Angélica Mesistrano, Comodoro regional, quienes participaron de todas las actividades de trabajo, turismo y sociales programadas.

En la cena de gala se efectuó el ascenso del Past Comodoro Chileno, Walter Eaglehurst, a Comodoro Regional Sud America y a Comodoro Regional al Comodoro área 2, una de las tres en que IYFR ha dividido administrativamente las flotas alrededor del mundo.



Entre las actividades realizadas se destacaron la navegación en el Catamarán Oceana desde el Club de Yates de Punta del Este a Punta Ballena, donde se pudo apreciar la maravilla arquitectónica creada por Carlos Paez Vilaró, “Casa Pueblo”, como también la especial visita al gran taller del Escultor Pablo Atchugarry, quien autografió trozos de mármol de Carrara de sus esculturas a los visitantes. Los asistentes contemplaron su extraordinario trabajo y sus monumentales esculturas de más de 20 toneladas que envía a las grandes capitales del mundo.

La gran sorpresa fue saber que el próximo país donde se realizará este especial encuentro de flotas Rotarias de Sudamérica será en Chile en el 2018 y la Organizará la Flota Chile Puerto Varas, con la misión de dar a conocer la ciudad y la Patagonia Chilena.

ROTAFLOAT NEWSLETTER COMPLIMENTS FOR THE PUBLICATION OF THE FANTASTIC ARTICLE

FELICITACIONES POR LA PUBLICATION A NOME DELLA ROTAFLOAT NEWSLETTER



Scouts Marinos Puerto Varas



Scouts Marinos

Editing & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant



Columbia

Cartagena de Indias Fleet



The new Cartagena de Indias Fleet

The city of Cartagena in Colombia located in the northern coast was known in the colonial era as Cartagena de Indias, founded in 1533. The city and its port was one of the most important ports of America, in the time of the Spanish empire. From this port all the riches of America were heavily guarded in the vaults of the city waiting for the arrival of the ships that would definitely transport them to the Kingdom from Spain. For that reason the city was always besieged by Pirates, who almost always failed in their attempt, due to the huge and strategic fortresses, land and submarines constructions to protect the city.

On the 11th of May 2017, in the halls of the Yacht and Fishing Club of Cartagena, Colombia 10° 25 '30 "north latitude and 75° 32' 25" west longitude, was given the birth of a new fleet named as Cartagena de Indias Fleet, increasing our Fleets and by so adding more to beloved fellowship IYFR. So we had celebrated at first for Colombia as well as for the wonderful Caribbean Sea.

The ceremony began with the interpretation of the national Hymn of Colombia, which was joined by the enthusiastic interpretation of the Rotary Hymn of Colombia.

Then the nominated Governor María Claudia Pérez, District 4271, had taken the floor to tell the invited for the ceremony about the foundation of the first IYFR Fleet of Colombia, in which she had special participation. María Claudia Pérez had welcomed the Area 2 Commodore Angelica Mesistrano and Regional Commodore and godfather of the fleet Walter Eaglehurst the name of the Colombian Rotary. María Claudia Pérez also presented her greetings to the Naval authorities headed by Vice Admiral Evelio Enrique Ramírez, of the Colombian Navy. She finished her emotional words by saying that she was proud to be part of the story of the birth of this new fleet.

Ex District Governor Roy Bozzi had followed by addressing the audience highlights of Rotary's achievements and achievements around the world and their importance today. The President of RC Cartagena had addressed his greetings to the visitors of the International Bridge.

Regional Commodore Walter Eaglehurst referred to the History of Rotary Fellowships, one of RI's most successful programs, and in addition to the beautiful history of our Fellowship.

Before the solemn moment of starting the charter ceremony of the new Cartagena de Indias Fleet, Area 2 Commodore Angelica Mesistrano, delivered her message to the entire audience, highlighting how wonderful the spirit of camaraderie and how it is transmitted through Rotary Fellowships, highlighting the importance of creating this new fleet, the first in the Caribbean Sea.

After completing her message, A2C Angie proceeds to invest the new Fleet Commodore Dr. Oscar Fontalvo, delivering the epaulettes of Fleet Commodore.

and receiving the sailor whistle honors, and the warm applause of the audience.

Then Fleet Commodore Oscar proceeded to invest his new Bridge: FVC, Amaury Benedetti, FRC, Roy Bozzi, FT Oscar Patron and FS Rafael Tono.

The newly invested Bridge of the new Cartagena de Indias Fleet could not conceal their joy, and than Area 2 Commodore Angie delivered the Charter card, and certificates to each of them and their respective burgees.

At that emotional moment FC Oscar Fontalvo, proceeds to hoist for the first time in the mast of ceremonies the new pennant of the Cartagena de Indias Fleet which had been followed by applause of all the present in the ceremony.

FC Oscar invited each of the Mariners of his fleet who one by one received the IYFR Pin, and also a Diploma by A2C Angie Mesistrano.

Once all the new fleet founding members had been invested, Fleet Commodore Oscar Fontalvo delivered his emotional message and delivered a reminder of the new Fleet to A2C Angelica Mesistrano and Godfather RC Walter Eaglehurst.

Later we shared a wonderful dinner after which the members of the Fleet Bridge turned out to be very cheerful enthusiasts and organized a beautiful sailing of six hours, aboard the sailboat "Pascal", of the FS Rafael Tono, in which they sailed bordering the coast of the beautiful historical city of Cartagena, to enjoy with music and songs having in hands rich saucers and Caribbean rum, and above all, much camaraderie and friendship.

The Cartagena de Indias Fleet Bridge



FT Oscar Patron, FVC Amaury Benedetti, FC Oscar Fontalvo,
FRC Ex Governor Roy Bozzi, FS Rafael Tono



RC Walter Eaglehurst, A2C Angie Mesistrano, RC Walter Eaglehurst, FM Sandra Gomez, A2C
FC Oscar Fontalvo, FT Oscar Patron Angie Mesistrano, FC Oscar Fontalvo

Nueva Flota Cartagena de Indias

Cartagena de Indias, fue el puerto más importante de América, en la época del imperio Español, todas las riquezas de América eran fuertemente resguardadas en las bóvedas de la ciudad a la espera de la llegada de los barcos, que la transportarían definitivamente al Reino de España. Por tal motivo la ciudad siempre era asediada por Piratas, los que casi siempre fracasaban en su intento, debido a las enormes y estratégicas fortalezas, terrestres y submarinas construidas para proteger la ciudad.

El 11 de Mayo , en los salones del Club de Yates y Pesca de Cartagena , Colombia 10° 25' 30" *latitud* norte y 75° 32' 25" de longitud oeste, se celebró el nacimiento de una nueva flota de nuestra querida agrupación IYFR siendo la primera de Colombia y la primera del maravilloso Mar Caribe.

La ceremonia comenzó con la interpretación del Himno nacional de Colombia, al que se le sumó la entusiasta interpretación del Himno Rotario de Colombia.

Luego la Gobernadora nominada María Claudia Pérez, Distrito 4271, hizo uso de la palabra para relatar la gestación de la primera Flota IYFR de Colombia, en la cual ella tuvo especial participación. Así mismo a nombre del Rotarismo Colombiano , dio la bienvenida a la Comodoro Area 2 Angélica Mesistrano y al Comodoro Regional y padrino de la flota Walter Eaglehurst. También entregó su saludo a las autoridades Navales presentes encabezada por el Vicealmirante Evelio Enrique Ramírez, de la Armada de Colombia. Terminó sus emotivas palabras diciendo que se sentía orgullosa de ser parte de la historia del nacimiento de esta nueva flota.

Posteriormente el Ex Gobernador de Distrito Roy Bozzi se dirigió a la audiencia para resaltar la obra y logros que Rotary realiza alrededor del mundo y su importancia en la actualidad. El Presidente de RC Cartagena también entregó su saludos a los visitantes del Puente Internacional.

El Comodoro Regional Walter Eaglehurst, se refirió a la Historia de las Agrupaciones de Rotary, uno de los programas más exitosos de RI , y en extenso a la hermosa Historia de nuestra Fellowship.

Llegado el momento solemne de dar inicio a la ceremonia de Charteo de la nueva Flota Cartagena de Indias, la Comodoro de Área 2 Angelica Mesistrano, entregó su mensaje a toda la audiencia, destacando que lo maravilloso que es el espíritu de compañerismo y cómo se transmite a través de las agrupaciones de Rotary , resaltando la importancia de la creación de esta nueva flota , la primera del Mar Caribe.

Una vez terminado su mensaje, la A2C Angie, procede a investir al nuevo Comodoro Dr. Oscar Fontalvo, entregando las epaulettes de Fleet Commodore.

Recibiendo los Honores de pito mariner y los calurosos aplausos de la audiencia.

Luego el Comodoro Oscar procedió a investir a su nuevo Puente : FVC, Amaury Benedetti, FRC,Roy Bozzi FT Oscar Patron y FS Rafael Tono.

El Puente recién investido, de la nueva Flota, no podía disimular su alegría, y la Comodoro de Área Angie entrega la Carta Constitutiva y los certificados a cada uno de ellos y sus correspondientes gallardetes.

En ese momento tan emotivo el FC Oscar Fontalvo, procede a izar por primera vez en el mástil de ceremonias el nuevo gallardete de la Flota "Cartagena de Indias".

Con el aplauso de toda la concurrencia.

Oscar invita a cada uno de los Mariners de su flota. Uno a uno reciben Pin de IYFR y Diploma por parte de A2C Angie Mesistrano.

Una vez investida toda la nueva Flota el Comodoro de la Flota Cartagena Oscar Fontalvo, entregó su emotivo mensaje y entregó un Gallardete recordatorio de la nueva Flota a A2C. Angelica Mesistrano y al Padrino RC.Walter Eaglehurst.

Posteriormente se compartió de una maravillosa cena.

Los miembros del Puente de la flota resultaron ser muy alegres entusiastas y organizaron una linda navegación de seis horas, a bordo del velero "Pascal", del FS Rafael Tono, en el que navegaron bordeando la Costa de la hermosa ciudad histórica de Cartagena, a bordo hubo música, cantos, ricos platillos, ron caribeño, y por sobre todo mucha camaradería y amistad.



Sailing in the bay of Cartagena. FM Sandra Gomez, A2C Angie Mesistrano, FC Oscar Fontalvo, FRV Ex Governor Roy Bozzi, FS Rafael Tono, RC Walter Eaglehurst, FVC Amaury Benedetti, FT Oscar Patron



View of old walled city Cartagena de Indias

Angelica Mesistrano
RC South America

Editing & Layout
Zeev Matar



Rotafloat Newsletter Editor / IC Special Assistant

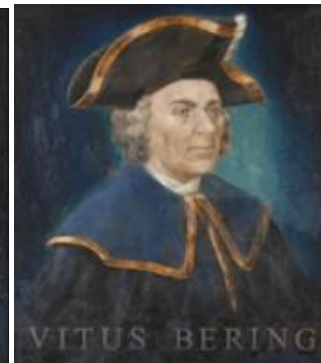
Bering Strait



The Bering Strait named in Russian as Proliv Beringa, is the strait connecting the Bering Sea with the Arctic Ocean. It is the strait which separates the continents of Asia and North America at their closest point of 85 km (53 miles). The Bering Strait is making part of the Bering Sea known by the Russians as Beringovo More is the northernmost part of the Pacific Ocean through the Bering Strait, where is stated the official boundary between the United States and Russia at 168° 58' 37" W.

The Bering Sea as well as the Bering Strait is named after Vitus Jonassen Bering, a Danish-born explorer in the service of the Russian Empire, who sailed into the strait in 1728 it lies slightly south of the Arctic Circle, being at about 65° 40' N latitude





Vitus Jonassen Bering known in Russia as Ivan I Ivanovich Bering was born on the 5th of August 1681 died 19th December 1741 He is known as a leader of two Russian expeditions, namely the First Kamchatka Expedition and the Great Northern Expedition, exploring of the north-eastern coast of the Asian continent and from there the western coast on the North American continent.

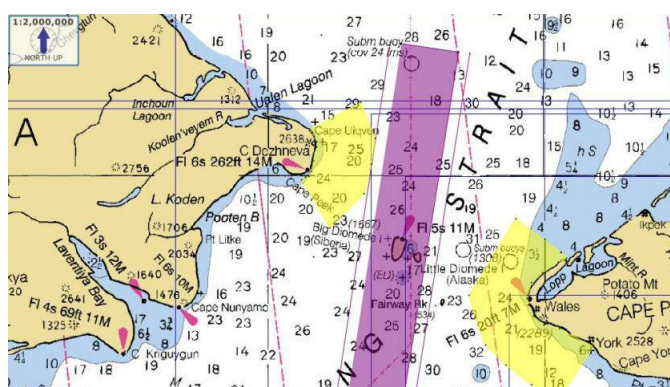
The Bering Strait, the Bering Sea, Bering Island, the Bering Glacier and the Bering Land Bridge were all named in his honor





Bering Strait data

The strait average depth is 30-50 meters (98-164 feet) and as mentioned its narrowest point is 85 km (53 miles). In the strait are located numerous islands, between them the two Diomed Islands of 16 square km (6 square miles) and in the south of the strait we find the St. Lawrence Island of 2,600 square km (1000 square miles)



Some of the Bering Sea water passes through the strait into the Arctic Ocean, but most of it returns to the Pacific. In winter the region is subject to severe storms and the sea is covered by ice fields averaging 1.2 to 1.5 meters (4 to 5 feet). In midsummer drift ice remains in the Bering Strait.

All of the Bering Sea water comes from the Pacific Ocean. The salinity of the surface water is relatively low, 31 to 33 parts per thousand; in the deeper parts of the sea the salinity increases to 35 parts per thousand near the bottom. In winter the northern portion of the sea is covered with ice, and even in summer the water below the surface retains a subfreezing temperature. The structure of the Bering Sea waters in general is subarctic, characterized by the presence in summer of a cold intermediate layer with warmer waters above and below. During the summer the surface water is heated, but a considerable layer of water that was cooled during the winter remains cold and is known as the cold intermediate layer. The maximum thickness of this intermediate layer is about 144.78 meters (475 feet) in the northern part of the sea and as much as 85.34 meters (280 feet) in the south. Underneath this layer is one that is slightly warmer, below which lie the colder bottom waters. In the northern and eastern shallow regions of the sea, only two upper layers develop: surface water and a cooler intermediate layer.

The warm oceanic waters from the south enter the Bering Sea through the numerous straits of the Fox Islands, through the Amchitka and Tanaga passes, and to a great extent through the Blizhny Strait between Attu and Medny islands. The Attu, Tanaga, and Transverse currents carry the warm water to the northwest. The Transverse Current, proceeding along the Asian continental slope in the direction of Cape Navarin, branches in two: one branch forms the Lawrence Current moving northward, and the other joins the Anadyr Current, which in turn gives birth to a powerful Kamchatka Current that governs the southward movement of the Bering Sea waters along the Asian coasts. Near the Alaska coast the general direction of the water is to the north, a factor responsible for the less severe ice conditions in that part of the sea as compared with the western part. Some of the Bering Sea water passes through the Bering Strait into the Arctic Ocean, but the bulk of it returns to the Pacific. The deep Bering Sea waters rise gradually to the surface and return to the Pacific as surface waters. Thus, the Bering Sea is an important factor in the general circulation of the northern part of the Pacific Ocean waters. The rise to the surface of oceanic waters rich in nutrient salts gives the sea a high biological productivity.



Climate

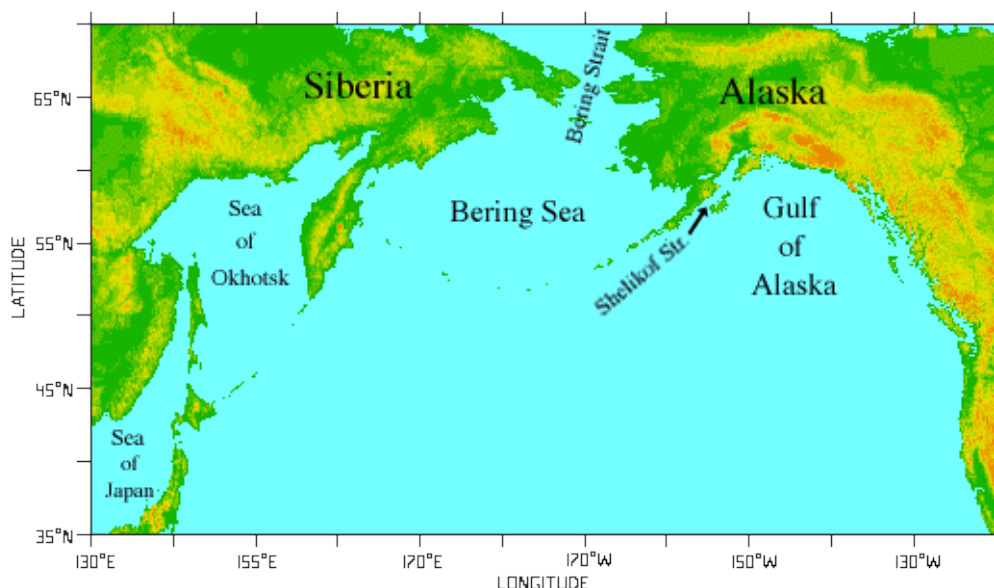
Although the Bering Sea is situated in the same latitude as Great Britain, its climate is much more severe. The southern and western parts are characterized by cool, rainy summers with frequent fogs and comparatively warm, snowy winters. Winters are extreme in the northern and eastern portions, with temperatures of -35° to -45° C (-31° to -49° F) and high winds. The summers in the north and east are cool, with comparatively low precipitation. Snow persists on the Koryak coast for as long as 8 months and on the Chukchi Peninsula for nearly 10 months, with a snow covers 0.60 meters (1-2 feet) thick. The annual precipitation in the southern part of the sea is more than 1,000 millimeters (40 inches), mainly in the form of rainfall, while in the northern part the precipitation is less than half as much and is mainly snow.

Water temperatures on the surface average from 1° C (34° F) in the north to 5° C (41° F) in the south. The period without frosts lasts for about 80 days in the northern part of the sea, where snow is common even in the summer and maximum temperatures are only 20° C (68° F). In the southern area there are nearly 150 days without frost, and the temperature seldom falls much below freezing. January and February are the coldest months, July and August the warmest. Severe storms caused by strong centers of low atmospheric pressure occasionally penetrate the southern part of the sea.



Strait & Sea Navigation

The Bering Strait & Bering Sea is considered to be one of the most difficult bodies of water to navigate. Winter storms are frequent and severe, often coating the superstructures of ships with ice. Wave heights may exceed 12.19 meters (40 feet). Added to these hazards are powerful tidal currents in many parts of the sea and fog, rain, and floating ice in the north. In winter the northern area is covered by ice fields about 1.20-1.50 meters (4 or 5 feet) thick, with hummocks in some places more than 30.50 meters (100 feet) high. At its maximum extent in April, the ice reaches as far south as Bristol Bay and the Kamchatka coasts. Melting begins in May, and by July there is no ice in the sea except for drift ice in the Bering Strait. Nonetheless, the sea contains important shipping routes for the Soviet Far East, including the eastern terminus at Provideniya on the Chukchi Peninsula for the northern sea route to Arkhangelsk in the west.



Bering Sea life

The existence in the Bering Sea of the cold intermediate layer separating the deep waters, which are rich in nutrient salts, from the upper layer (the layer exposed to sunlight) which results in two growths of floating plant life during the year. The first growth occurs in the spring after the mixing of waters in winter, and the second during the autumnal mixing, when the cold surface waters descend and the deeper waters come to the surface while there is still sufficient sunlight for plant growth.

The Bering Sea has more than 300 species of fish, including 50 deep-sea species, of which 25 are caught commercially. The most important among them are salmon & herring. The islands are breeding grounds for the fur seal and the sea otter. The northern areas are inhabited by the walrus, seal, and sea lion. Several whale species, notably gray whales, migrate to Bering waters to feed during the summer. Intensive fishing in the last half of the 20th century has drastically reduced some of the most valuable fish species, and this has led to greater exploitation of less commercially valuable species.



Little Diomede Island (left) and Big Diomede Island, Bering Strait



Little Diomede, Bering Strait



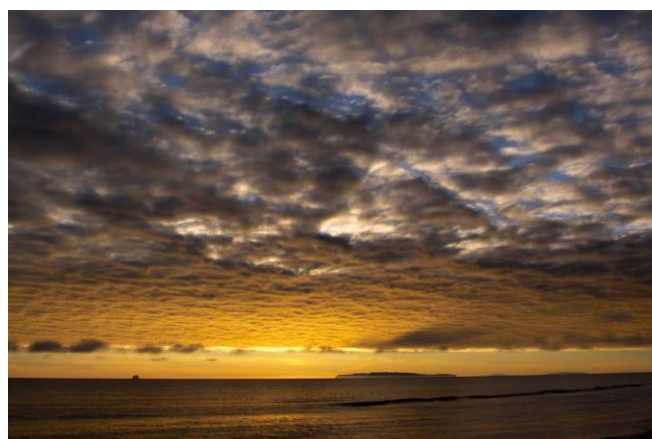
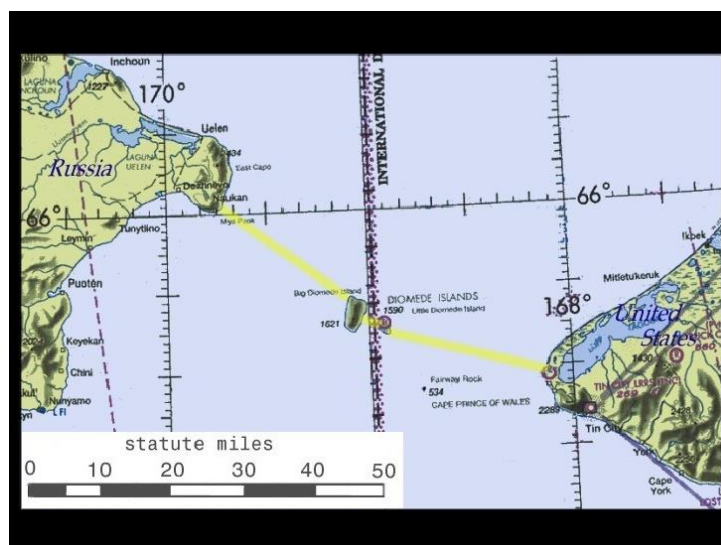
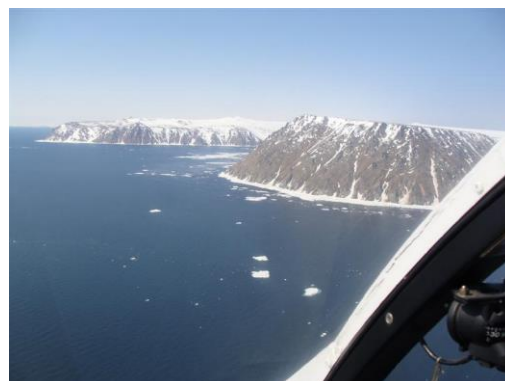


This is a picture of the Bering Strait itself. The Bering Strait is a sea strait between Cape



View of the Bering Strait and the mountains of Siberia.







Little Diomedede and Big Diomedede Islands

Yacht sailing in the Bering Strait

These two islands are situated in the middle of the Bering Strait 3 km apart. The Russian - American border (also the International Date Line) runs between them. The smaller of these granite islands is American. These islands can be used as a resting point during a crossing of the Bering Strait; however, being in the proximity of Russia's Big Diomedede Island will reduce the chance of a successful crossing from Alaska to Russia. It is unlikely permission will be obtained from the Russian government to cross the Bering Strait, which means a successful crossing involves keeping a low profile. There is a heavy military presence on Big Diomedede and the soldiers will turn back any explorers trying to sneak into their country. From what we've heard they are pretty good natured, and are unlikely to shoot.

AsianYachting - August 2007 News and Views

First Vertical Circumnavigation via Russia's northern Polar route

Alpha Global Expedition: Adrian Flanagan sails where no solo yachtsman has ever been before. His yacht *Barrabas* has departed on the Russian route no single-handed yachtsman has ever been given permission to sail before. Setting sail from Provideniya on Monday 23rd July at 1100 am local time after a brief but hugely reassuring visit to clear Russian customs and gain the final permit to travel Russia's northern Polar route. The weather was clear and fine as a high pressure system continues to sit over the Bering Strait. This stretch of water can be dangerous with fast currents running in both directions between the Chukchi Sea in the north and the Bering Sea to the south. He owes a great many people a huge debt, especially Louise and John Mann in Moscow plus Roman Abramovich, governor of Chukotka for working so hard to secure permission from The Russian Administration to allow him to attempt this unique voyage. Success in life is never guaranteed, but one thing is assured - *Barrabas* and I will give this everything we have. Follow the expedition at <http://www.alphaglobalex.com>



British single-handed polar circumnavigator Adrian Flanagan



Russia Magadan Region Chukotskiy



BRITISH YACHTSMAN COMPLETES FIRST EVER 'VERTICAL' CIRCUMNAVIGATION OF THE GLOBE

Posted on [May 20, 2008](#) by [admin](#)



Adrian with crystal polar bear presented to him in Murmansk

Intrepid sailor, Adrian Flanagan will sail in to Southampton Water tomorrow, the first person to complete a singlehanded 'vertical' circumnavigation of the globe, westwards around Cape Horn and across the Russian Arctic Coast. Adrian's challenge to set the record for sailing round the world 'over the top' has tested the limits of his endurance. He is the only yachtsman to have ever sailed Russia's Arctic Coast along the Northern Sea Route single-handed. Amongst many adventures, Adrian has been washed overboard, dislocated both wrists, suffered two knock-downs at Cape Horn and been tracked by pirates off Brazil, but tomorrow he will sail up the River Hamble to a tremendous welcome from family and friends.



Meet and Greet Arctic style, Walrus float past Barrabas near the remote island of Ostrov Peschanyy

British yachtsman, Adrian Flanagan, 47 from Buckinghamshire will arrive at the Royal Southern Yacht Club, Southampton on Wednesday morning to complete the first single-handed 'vertical' circumnavigation. Flanagan will have sailed more than 30,000 miles on his 40-foot Stainless Steel yacht Barrabas in pursuit of a boyhood dream inspired after he read Sir Francis Chichester's 'Gipsy Moth Circles the World'. Flanagan set sail in October 2005. Sir Robin Knox-Johnston who became the first man to sail around the world alone without stopping described Flanagan's voyage as, 'seriously difficult'.



Barrabas approaches Diamond Head Buoy off Oahu

Flanagan was forced to break his voyage twice. He said, 'My route westwards around Cape Horn and along the Russian Arctic coast had never been done before. The delays reflect the degree of difficulty, not only physical but political.' Only six yachts have been given permission by the Russian authorities to travel the Northern Sea Route. Two of those were Russian. All were crewed and carried a mandatory Russian ice-pilot onboard. Flanagan wanted to go it alone – single-handed and without an ice pilot. Bureaucratic delay forced Flanagan to lay up his yacht in Nome, Alaska during the winter of 2006-7. He eventually won permission to go into the militarily sensitive areas of northern Russia's Arctic waters in July 2007. Barrabas became the first British flagged yacht to go there. Flanagan commented, 'True adventure requires a unique aspect, something pioneering, something that's never been done. To have become the first solo yachtsman to enter the Russian Arctic is irreducible. It is the crowning achievement of the voyage, not just for me but for everyone involved.'

Article & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant



International Yachting Fellowship of Rotarians



Dedicated poem to our Fellowship in Japan

アイフォーリアンの詩

ロータリーヨットの友よ

私達は海の王達

風を帆に受け湖や海を渡る

私達は広く異なる旗の下、

強く結ばれたフリート

どんな国でもすべての海と恋をする

友よ、友情の帆でセーリングに行こう

兄弟愛は私達の言葉

ウオッカとシャンパンは永遠のトロフィー

ワインとラムは体を駆け巡る血

皆に、ビバ、アイーサ、マブハイ

アイフォーリアンすべてに平和を

シャロームは私達の言葉、アーメン

優勝杯を目指してセーリングに出よう

ヨットは私達の共通の愛

そして、私達は運命づけられたセーラー

曇天や輝く光の中へ

セーリングに行こう

マリナーズとオフィサー達

無限の楽しみを持とう

ビバ、ロータリーヨットの友よ、舵輪を廻せ

ギリシャのポセイドンに祝福され

ヨットと風に挑戦する

そして私達に常に勝利を

作：ジープ・マタル

訳：梶野興三

Zeev

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

Credit Poem translation to RC Kozo Kajino





JAPAN IYFR AREA 3 AGM KOBE

INTERNATIONAL YACHTING FELLOWSHIP OF ROTARIANS



I.Y.F.R. KOBE FLEET JAPAN



IYFR AREA 3 AGM 8-13 November 2017 Kobe, Japan

The Schedule

Wednesday 8 November

15:00

- Arrival and check in at **Kobe MerikenPark Oriental Hotel**

17:30 ~ 18:30 Bar time

- Informal meeting & greeting followed by dinner

18:30 ~

- **Kobe welcome dinner**



Thursday 9 November

9:00 Depart Hotel

- Sightseeing in Kyoto city by a chartered bus

11:00~12:00 Kinkaku-ji Temple

12:10~13:00 **Lunch**

13:10~14:00 Ryoan-ji Temple's rock garden

15:00~ Kiyomizu-dera Temple etc.

17:30~20:00

- **KYOTO NIGHT!** Enjoy Japanese "KAISEKI" dishes with Kyoto-Lake Biwa Fleet members, appreciating Maiko's performance.



Friday 10 November

9:00~12:00

- **AGM** in morning

Those not attending AGM can go shopping (guided or independent) or sightseeing. Kobe Fleet members are available for transport, if needed.

- Free time in afternoon



19:00~21:00

- **KOBE NIGHT!** Enjoy Formal IYFR dinner with Kobe Fleet members.

Saturday 11 November

8:00 Depart Hotel

- Sightseeing in Hiroshima city by a chartered bus

12:00~13:00 Lunch

14:00 Visit Atomic Dome and Peace Memorial Park.



16:30 check in **Hotel SunRout HIROSHIMA**

18:00 ~

- **Hiroshima NIGHT!** Enjoy dinner with Setouchi Shimanami Fleet members.

Sunday 12 November

8:30 Depart Hotel

- Sightseeing Onomichi or Beautiful inland see coast by bus

11:30~12:30 Lunch

- After lunch, short cruise (now planning)

After cruise, coming back to Kobe. (17:30)



18:30~

- **Farewell NIGHT!** Enjoy freely your last night in Kobe.
- We'll arrange some local restaurants and Karaoke , if needed.

Monday 13 November

- Official IYFR event at an end but if delegates wish to travel to other places in Japan. there will be plenty of information available but members need to make their own arrangements.

IYFR KOBE FLEET IS WAITING FOR YOU!

The expense (including 5 night accommodations and, all meals, sightseeing, coach fee guide costs etc.) is 140,000 yen per person.

Sanyo Air Service
<http://www.travelsas.co.jp/>
 TEL:0120-66-3459

organizer

Michinobu KOYAMA(Secretary of KOBE Fleet and head of Sanyo Air Service)
 Kozo Kajino (Regional commodore for Japan) 2017.6.28

IYFR Area3 AGM in Kobe, Japan

2017 年 11 月 8 日 (水) ～ 2017 年 11 月 13 日 (月)

作成日：2017 年 6 月

30 日

| 取 | 月日(曜) | 地 名 | 現地時刻 | 交通機関 | 予 定 (宿泊地) | 食 事 |
|---|---------------|------------|---------|------|--|-------------------|
| 1 | 11月8日 (水) | 神 戸 | | | 2017 AGM 受入開始 チェックイン 15:00～ BARタイム 17:50～18:50 KOBÉ ウェルカムディナー 18:50～ (ホテル・グランドパレス) | 昼：－ 夕：○ |
| 2 | 11月9日 (木) | 神 戸 | | 貸切バス | 京都市内観光 金閣 南禅寺石庭 清水寺 等 京船の夕べ 南禅寺「八千代」 17:50～19:50 バスで神戸へ (ホテル・グランドパレス) | 朝：○ 昼：○ 夕：○ |
| 3 | 11月10日 (金) | 京 都 | | | AGM会議 9:00～12:00 (ホテル 海王の間) 神戸市内 神戸の夕べ ～ ディナークルーズ 19:00～21:00 (ホテル・グランドパレス) | 朝：○ 昼：○ 夕：○ |
| 4 | 11月11日 (土) | 広 島 | 9 : 0 0 | 貸切バス | 広島へ 原爆ドーム 平和公園 瀬戸内の夕べ (リムルート広島) | 朝：○ 昼：○ 夕：○ |
| 5 | 11月12日 (日) | 広 島 神 戸 | | 貸切バス | 尾道しまなみ ガーゼン & ツチ 又は 牛窓クルーズ＆ランチ (ホテル・グランドパレス) | 朝：○ 昼：○ 夕：○ |
| 6 | 11月13日 (月) | 神 戸 | | | チェックアウト 解散 | 朝：○ 昼：－ |

※この日程表は最も新しい資料により作成しておりますが、交通機関の都合により出発到着時刻の変更が生ずる事があります

宿泊代及び移動費用と食事マークの○は費用に含めます。

全費用 140,000 円 予約金 60,000 円 〆切 7 月末日

◎ 詳しくはロータフロード掲載の英文を参照下さい。

主催 IYFR 神戸フリート

世話人 香山道宣 (神戸フリート 三洋航空サービス)

梶野興三 (京都・琵琶湖フリート PRC)



International Yachting Fellowship of Rotarians



Dedicated poem to our Fellowship in the Philippines

Ang Tula ng mga IYFORIANS

Mga Mandaragat na Rotarian
Tayo ay mga hari ng karagatan
Tumatawid sa alinmang lawa o dagat
Namamayagpag na layag sa hangin
Nagkakaisang pangkat
Pumapailalim ng iba't ibang watawat
Sa anumang Kabayanan
Nagmamahal sa lahat ng mga karagatan
Maglayag mga kaibigan
Maglayag sa pagkakaibigan
Kapatiran ang ating wika
Vodka at Champagne
Ang ating Tropeo
Alak at Rum
Ang tatakbong dugo sa ating mga ugat
Viva at Aisa
Mabuhay sa lahat aking mga kaibigan
Kapayapaan sa inyong lahat IYFORIANS
Shalom ang ating wika, Amen
Maglayag mga Marinero
Sa maulap na panahon o sa
Sikat ng araw
Sige at lumayag
Marineros at Opisyaes
Sa walang hanggang kasiyahan
Mabuhay ang samahan ng mga Mandaragat na Rotarian
Isulong ang gulong na
Binasbasan ng Griyegong Poseidon
Sa hamon sa ating mga bangka at hangin ng karagatan
At sa tuwinang Pagtatagumpay

Zeev

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

Credit Poem translation from English by Guia Avecilla



Philippines Region

Philippine Bridge Changes Command in Atlanta

Regional Commodore Oyie Valarao has decided to give way and end her term coterminous with IC Jun Avecilla on June 30. Taking her place is Regional Vice Commodore Edmond Aguilar who was installed by Outgoing IC Jun, Incoming IC Guillermo Arteta and Chief of Staff Sergio Santi. Also installed were FC Ernest Yuyek of the Philippine All Star Fleet who moves up to the RVC Post and FC Jim Baniqued who will assume command of the Cubao West Philippine Fleet. Edmond was Past District Governor of District 3810 while RVC Ernest is the current Governor. FC Jaime Baniqued is the incoming President of RC Cuba West.

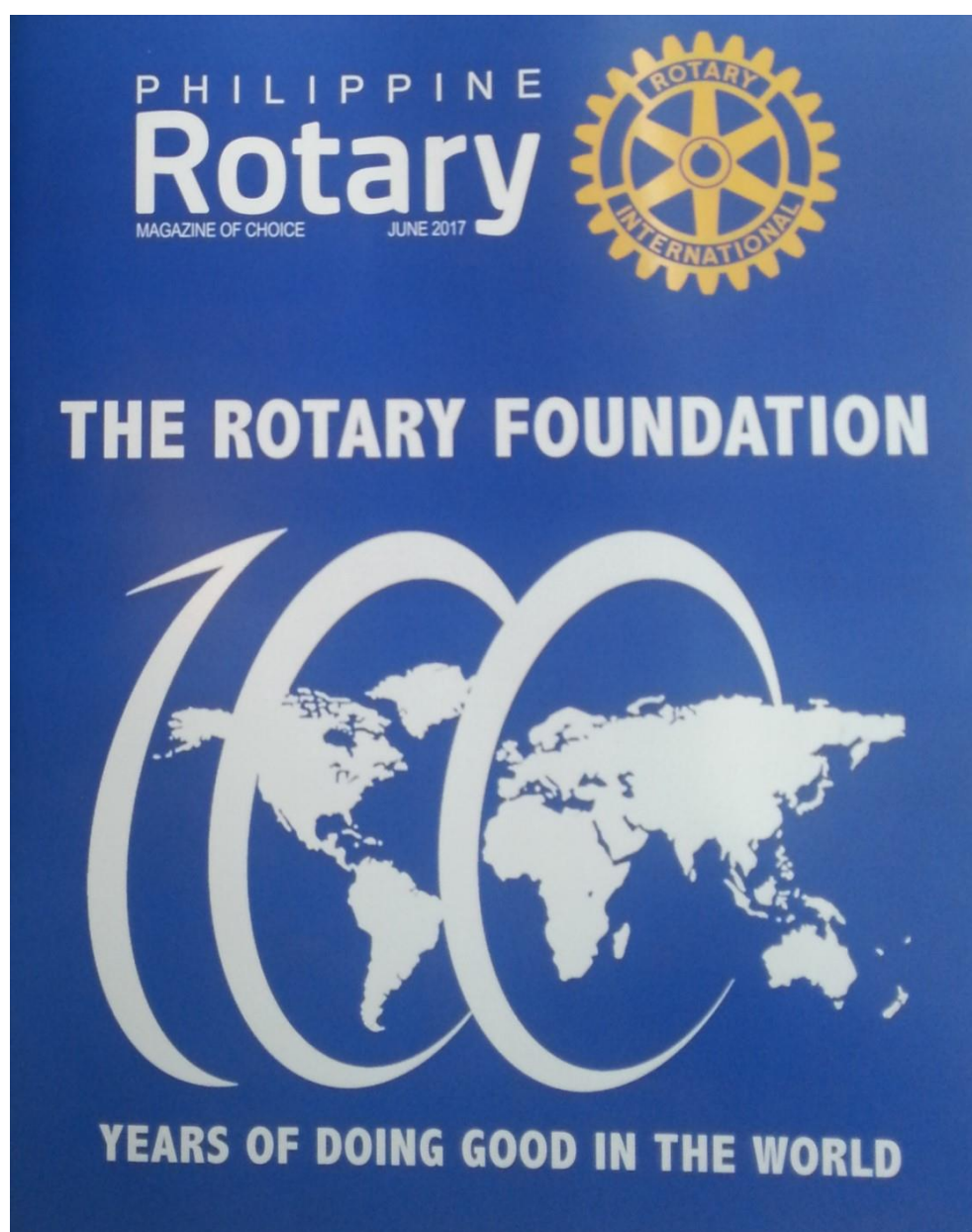
RC Oyie Valarao has been responsible for the exceptional growth of the Philippine Region beefing up the Fleets to 27. For her extraordinary feat IC Jun expressed his thanks and gratitude and presented her with an IC Award during the Gala night. IC Jun expressed that RC Oyie could do well to work for the development of Fleets in South East Asia and has recommended Incoming Commodore Guillermo to tap and utilize her potential by putting her in his International Bridge and be given an active role in Area 3. With RC Edmond and RVC Ernest they form a formidable team to beef up the fleet not just in the Region but in the whole Area. Bridge Treasurer Loida Gascon who has helped in the AGM organization and IC Jun's sister Guia who was the Atlanta AGM Chair with her Husband Armando received a token of Appreciation from IC Jun for their invaluable roles.





Philippine Rotary Magazine June 2017 Issue

Rotafloat Newsletter Magazine is pleased to offer the Philippine Rotary Magazine which had included in its June 2017 Issue a large service about our International Yachting Fellowship of Rotarians. The publication giving such cover to IYFR activity in the Philippines is first of all the merit of our IC Jesus "Jun" Avecilla the founder of the First Cubano Weat Fleet in the Philippines later spread by his activity to create and charter other 10 Fleets while RC Gloriosa "Oyie" V. Valaro had created 20 Fleets during 2015-2017 mandate as Philippines Region Commodore.



International Yachting Fellowship of Rotarians

Jesus “Jun” Avecilla
International Commodore



*DG Dwight Ramos with IYFR International
Commodore Jesus “Jun” Avecilla during the
Rotary Golden Wheel Awards Night*

IYFR: An Extraordinary Fellowship





PP JUN AVECILLA:

First Asian and Filipino INTERNATIONAL COMMODORE OF THE IYFR

PP Jesus “Jun”E. AVECILLA of the Rotary Club of Cubao West, RI District 3780 made history in Rotary’s international fellowships when he was installed as Commodore of the International Yachting Fellowship of Rotarians (IYFR) during the Sao Paulo International Convention held on June 6-9, 2015. He was the first Filipino ever to be given that top IYFR post and the 38th International Commodore in its 69 years of experience. He succeeded International Commodore Sergio Santi of Italy who served in 2013–15. IPIC Santi turned over the leadership in ceremonies held during the IYFR Area General Meeting (AGM) attended by IYFR officers and mariners around the world. At the same event, CP Gloriosa “Oyie” Valarao was elevated to Regional Commodore and took over the leadership of the Philippine Fleet from RC Udo Pelkowski.

IYFR is the oldest, largest, and most prestigious fellowship group in the Rotary world founded in 1947 with more than 110 fleets and 3,300 members spread in 37 countries. In a recent interview, IC Jun emphasized that the Fellowship *“operates in accordance with RI policy but is not an agency of, or controlled by RI. However, with the ideals of Rotary and as Rotarians ourselves, IYForians (as IYFR members are called) remain steadfast in supporting the good causes and events, service projects and opportunities of our Rotary clubs, on and off the water. Our dedication is to promote Rotary fellowship and understanding among nations and our members, through our love of sailing and activities on the water and advance international understanding, goodwill and peace through a world fellowship of business and professional persons united in the ideal of service.”*

IC Jun further describes IYFR as a *“RI-recognized and sanctioned conglomeration of Rotarians all over the globe having the natural affinity for marine life, the intrinsic and inherent concern for the preservation and conservation of the diversity of all its water resources.”* This is one beautiful reason the IYFT Philippine Fleet has focused its projects on beach clean-up, tree planting along riverbanks and in mangroves, rescue flood victims during severe typhoons, donation of motorized boats to fisherman to help rebuild their lives devastated by super typhoon “Yolanda,” and many other projects that help people live the good life they dream.

Asked how an individual can join IYFR, Jun puts the basic qualification for membership as:

- A rotation must be in good standing, and
- Must have a natural and inherent affinity for marine life



and its basic habitat... water... sea... brackish, river, lake water, its tributaries and myriad waterways. He also emphasized that a Rotarian does not need to own a yacht to become a member.

IC Jun founded the Cubao West Fleet, which was chartered on March 18, 2009 in ceremonies held at the Manila Yacht Club. This move became a nautical milestone in the history of RI District 3780 as it paved the way for IYFR's expansion in the Philippines. The Cubao West Fleet spearheaded the formation of The Philippine Fleet, now a part of IYFR Area 3 composed of Asia, Australia, New Zealand, and South Pacific, and became the mother of all fleets in the Philippines, which today have grown to 10 fleets. The Philippine Fleet has set up its headquarters at The Lighthouse Marina Resort, Moonbay Marina Complex, Waterfront Road, Subic Freeport Zone, Subic Bay, Zambales.

IC Jun is a man of great and undying passion for adventure especially at sea. As an avid sailor, he sails and races with the best in the world. He has won many Regattas where he competed and skippered his own yacht, Selma Star, a Beneteau First 36.7 racer cruiser. The international races he won include the Singapore Straits Regatta in 2006, the Borneo International Yachting Challenge Regatta, which he ruled in five consecutive years (2007-2012) and The Subic Bay to Boracay 200-mile Offshore Race and the Boracay Cup Regatta in 2006, 2009, and 2013. His adventurous spirit drove him to organize the Cubao West Fleet and consequently the Philippine Fleet and to selflessly serve as the editor of the IYFR Rotalog, the official newsletter of IYFR Philippines. These are some of the beautiful reasons after serving as Philippine Regional Commodore in 2009-11: Jun was installed as International Rear Commodore in 2011-13, and International Vice Commodore in 2013-15. Following his installation at the Sao Paulo International Convention, he now serves as International Commodore for 2015-2017.

The growth of IYFR in the Philippines was highlighted when it set the stage for the IYFR Area 3 General Meeting on February 3-8, 2016 in Auckland, New Zealand. With RC Oye at the helm, the Philippine Regional Command organized the grand fellowship on January 31, 2016 welcoming IPIC Sergio Santi and his Spouse Laura when they visited our country on their way to the Auckland AGM, celebrating the leadership of IC Jun, and serving as send-off party for IYFR mariners who attended the Auckland event.



From left – Gloriosa “Oyie” Valarao, RC Philippines / IC Special Assistant, FC Edmond Aguilar, FVC William Rogando, and FRC Johnson Tan.



The chain of events paved the way for the elevation of CP Oyie Valarao to Philippine Regional Commodore after serving as Regional Vice Commodore in 2011-15. Driven by her passion for Rotary, she immediately left no stones unturned in organizing the Dragon Fleet of RI District 3810, the first ever fleet that laid the IYFR conquest of the said district. The formation of the Dragon Fleet was spearheaded by then District Governor Edmond “Mond” Aguilar who became its Charter Commodore and the world's first ever current governor to become the Commodore of an IYFR fleet.

The Dragon Fleet was only the beginning of a master plan hatched by RC Oyie for her leadership in 2015-17. She has planned to put up at least three fleets in every Rotary district to triple the number of fleets from 10 to 30 – a dream that is possible to achieve given her burning passion for success. With the full support of IC Jun, she has now begun to beef up her organization with the appointment of seven Regional Commodores: RVC Gerry Buhain (D3780), RVC Edmond Aguilar (D3810), RVC Francis Rivera (D3790), RVC Pichoy Ramirez (D3770), RVC Joe Ragos (D3800), RVC Udo Pelkowski (D3860), and RVC Ernest Yuyek (RVC-at-Large).

These RVCs have stepped up efforts to organize new fleets in their respective posts resulting in the creation of new IYFR fleets.

(Source: Article written by Philippine Fleet Bridge PRO Bob Autor, published in Rotalog Newsletter - July to September 2015 issue no. 99)



The year 2017 will have more IYFR Events to attend than any other time in the history of IYFR. *We are an International Fellowship and our first two objectives are;*

1. **Help to advance Rotary's international understanding, goodwill, and world peace**
2. **Meet interesting people, make great contacts, and establish lasting friendships.**

2017

So consider the following events and maybe plan your 2017 vacation around one of the events listed.



June 9 - 15

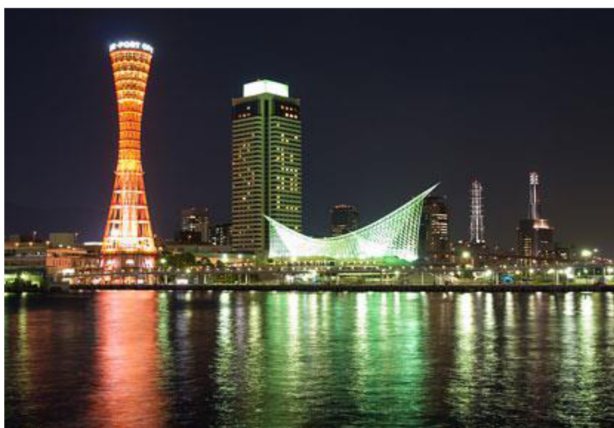
Atlanta Georgia USA. IYFR AGM and Change of Watch in conjunction with the Rotary International 2017 Convention. IYFR hotel is the JW Marriott Buckhead which is on the RIC bus route.

Status: Registration is open, please contact IC Jun Avecilla at selmajea@mozcom.com to receive the Registration Form, Programme, and costs. Room deposit is required after booking confirmed.

June 15 - 19

Pensacola Florida USA. IYFR PCC (Post-Conference Cruise). The hotel will be the Pensacola Grand which is ideally located down town minutes away from shopping, dining, and entertainment venues.

Status: Hotel bookings are now open; please contact IPRO Roger Lingard at lingardrg@bigpond.com to reserve your room for \$US185pn. The programme should be ready by October this year. No payments are required until invoiced.



November 8 - 13

Kobe Japan. A3GM (Area 3 General Meeting. This event is in the planning stage with more details available in November this year. It is being co-hosted by the Kobe and Kyoto Lake Biwa Fleets. Western Australians are considering some pre-meeting fellowship in Bali.

For more information, email lingardrg@bigpond.com

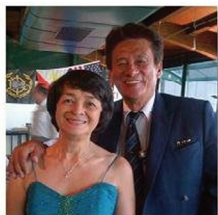
There are more events planned. Organisers, please email me information so I can circulate to all members. Thank you.

Roger Lingard
Western Australia
lingardrg@bigpond.com



INTERNATIONAL YACHTING FELLOWSHIP OF ROTARIANS

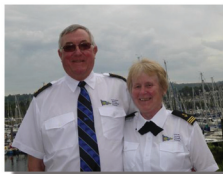
PROMOTING FUN AND FELLOWSHIP WORLDWIDE.



COMMODORE JESUS 'JUN' AVECILLA
AND FIRST MATE LIL
MANILA, PHILIPPINES



VICE COMMODORE GUILLERMO ARTETA
AND FIRST MATE MONICA
BUENOS AIRES, ARGENTINA



REAR COMMODORE ROBERT "BOB" BURNS
AND FIRST MATE ANN
WEMYSS BAY, UNITED KINGDOM



AREA 3 COMMODORE KERRAN CAMPBELL
AND FIRST MATE CAROL
PERTH, AUSTRALIA



REGIONAL COMMODORE GLORIOSA 'OYIE' VALARAO
PHILIPPINE REGIONAL COMMAND
MANILA, PHILIPPINES

International Yachting Fellowship of Rotarians Philippine Regional Command

RC Gloriosa O. Valarao

RVC Edmond Aguilar

RVC Gerry Buhain

RVC Francis Rivera

RVC Joe Ragos

RVC Pichoy Ramirez

RS Melchor Waje

RT Elvira Bautista

- | | |
|----------------------------|---------------------------|
| 1. All Star Fleet | FC Eddie Toribio |
| 2. Cebu Fleet | FC Sven-Olof Tengelin |
| 3. Cubao West Fleet | FC Alex Bernales |
| 4. Dragon Fleet | FC Andrew Co |
| 5. First Class Fleet | FC Fernando Pine |
| 6. Fishers of Man Fleet | FC Johnson Tan |
| 7. Hands On Fleet | FC Linda Palattao |
| 8. Kagitingan Fleet | FC Winston Ongchinke |
| 9. Mabuhay Fleet | FC Kenneth Terrado |
| 10. Neptuno Marinero Fleet | FC Marie Grace Macatangay |
| 11. Pacific Shore Fleet | FC Maria Gina Sy |
| 12. Pacific Wave Fleet | FC Everett Olivan |
| 13. Peace Fleet | FC Michael Crisologo |
| 14. Rainbow Fleet | FC Dennis Arca |
| 15. San Remigio Fleet | FC Antonio Ynoc |
| 16. Sea Horse Fleet | FC Rolando Jesse Briosos |
| 17. Service Fleet | FC Bernadette Herrera Dy |
| 18. Southern Wind Fleet | FC Erhard Larga |
| 19. Stingray Fleet | FC William Rogando |
| 20. Subic Bay Fleet | FC Zedrik Avecilla |
| 21. The Mermaids Fleet | FC Violeta Giron |
| 22. World Class Fleet | FC Arturo Supangan II |

"The members of the IYFR are not only boat lovers and sailing enthusiasts but also ecology conscious Rotarians who are concerned with having a clean and healthy environment particularly in the open seas, the coastal areas, the inland waterways and their surrounding sites."

-- PP Ping Sison, IYFR Regional Legal Adviser, in his column A LAW EACH DAY (KEEPS TROUBLE AWAY) column, Philippine Star, November 7, 2014



Rotary

Club of Cubao West



Meets Every Wednesday, 7:30 PM , Stoffel's Room, Hotel Rembrandt T. Morato Extension, Quezon City

Sailboat Basic Terms

Essential Words You Need to Know



3rd article

Cockpit

The outdoor area of a sailing yacht typically positioned in the stern where are positioned the controls of a yacht where the captain steer and control the boat. It is traditionally an open well in the deck of a yacht outside any deckhouse or cabin which is also used as the area. In modern yachts the cockpit refers to enclosed area for the sailing team to sit and eat.



In smaller boats have aft cockpit towards the stern while in larger yachts we can find center cockpit providing a greater weather protection.

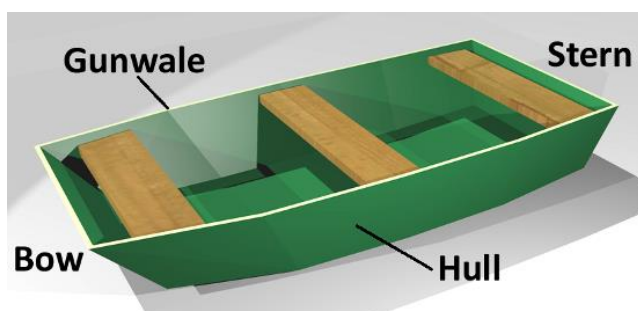




Center cockpit

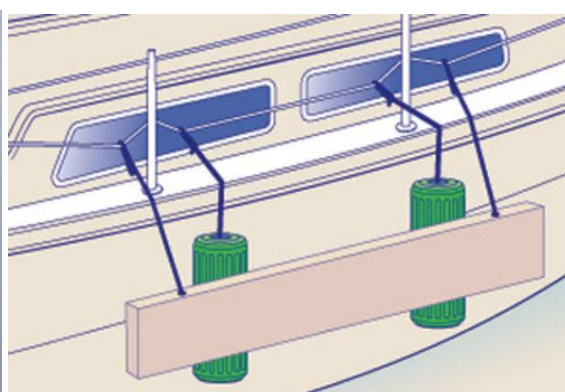
Gunwale

The term refers to the upper edge of the side of a boat which originally was the “gun ridge” on a sailing warship. The gunwale represents in modern yachts the stiffening structural band in the yacht. The outer edge of the boat’s deck and cockpit is also called the rail



Fender

A bumper hung alongside to prevent the hull from rubbing against a dock or other moored sailing yacht. The fenders have the quality to absorb high energy and low reaction forces. The fenders are produced from elastomer rubber foam or from plastic made in different shapes and sizes.





Fender Step – Boarding step Fender



Fender Racks

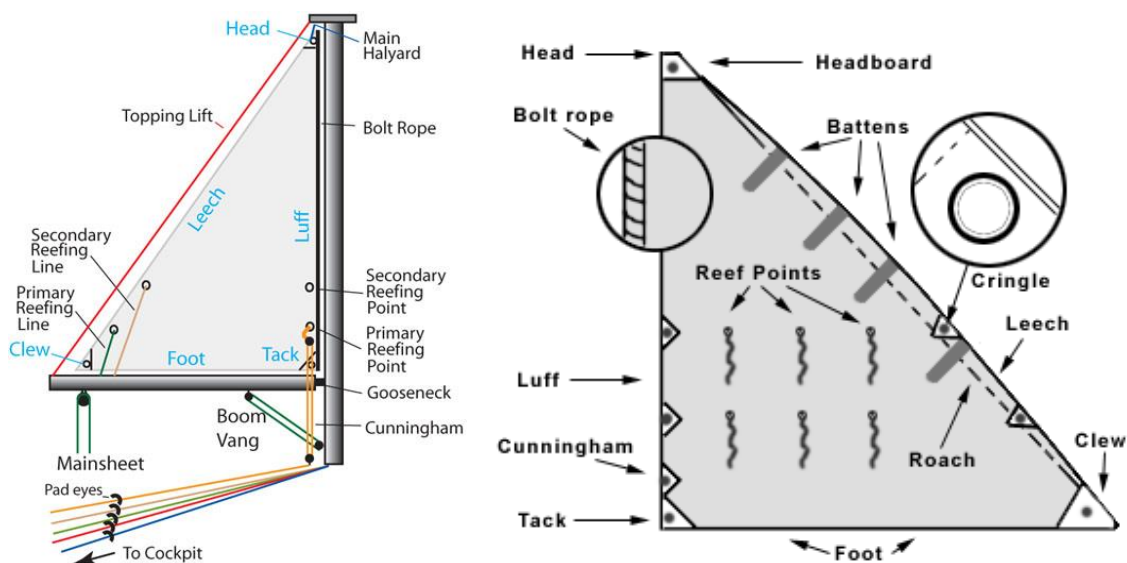


How to Hang a Fender on a boat

Foot

The foot is the bottom edge of a sail. On a fore-and-aft mainsail, the foot is often attached at the tack and clew to a boom, on square sail to a spar by clews on both ends. If no boom or spar is present the sail is said to be “loose-footed”

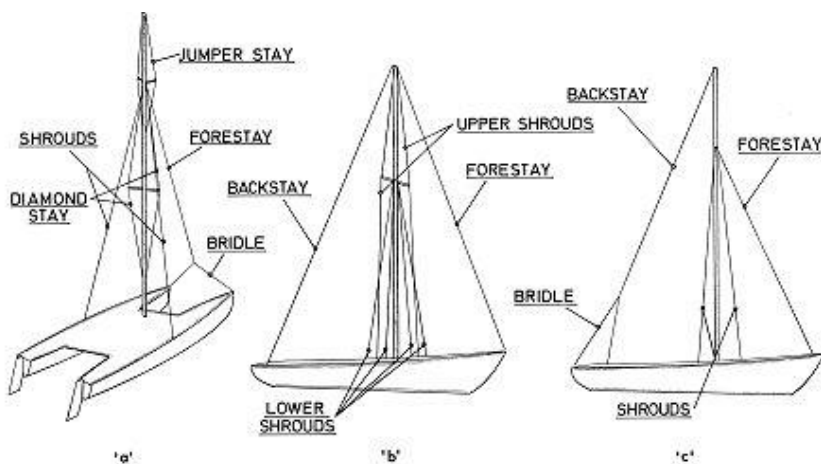
Spar – is a pole of wood, metal or lightweight materials such as carbon fiber used in a sailing yacht to carry or support its sail.



Forestay - Stay

This is the cable from the bow to the masthead, which is a piece of standing rigging (lines, wire, rods) which keeps a mast from falling backwards. The forestay is attached either in very top of the mast or between 1/8 and 1/4 from the top mast while the other end of the forestay is attached to the bow of the yacht. Standing rigging is placed under tension to keep each mast (or bowsprit) securely in position and adequately braced to handle loads induced by sails.

The forestay is made of stainless steel wire or solid stainless steel rod, carbon rod or ultra high molecular polyethylene (UHMWPE, UHMW)



Standing rigging is placed under tension to keep each mast (or bowsprit) securely in position and adequately braced to handle loads induced by sails.

Standing rigging term is used in contrast to running rigging of a sailing yacht which are used for raising, lowering, shaping and controlling the sails.

Furling

Rolling or folding and reefing (reducing the area of the sail) as packing a sail by rolling it around a stay or rotating spar in case of jib sail while

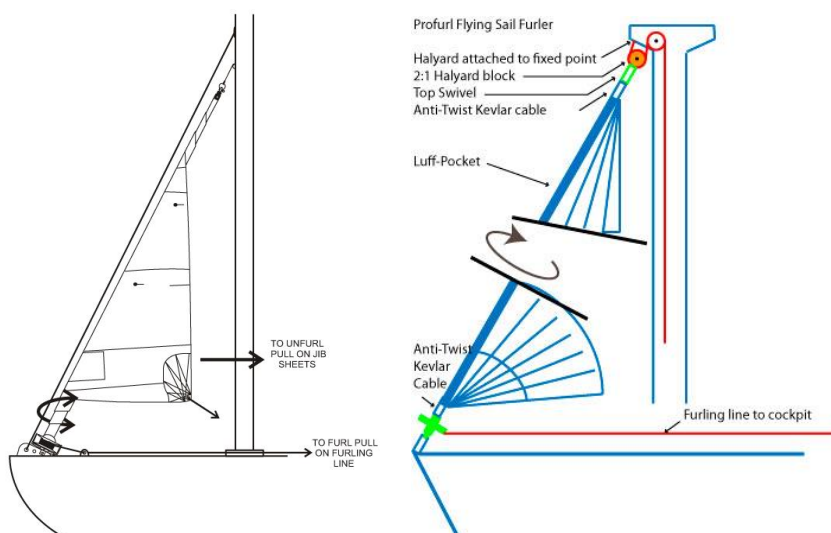
Roller furling is most prominent on foresails such as jibs, followed by mainsails reefing.

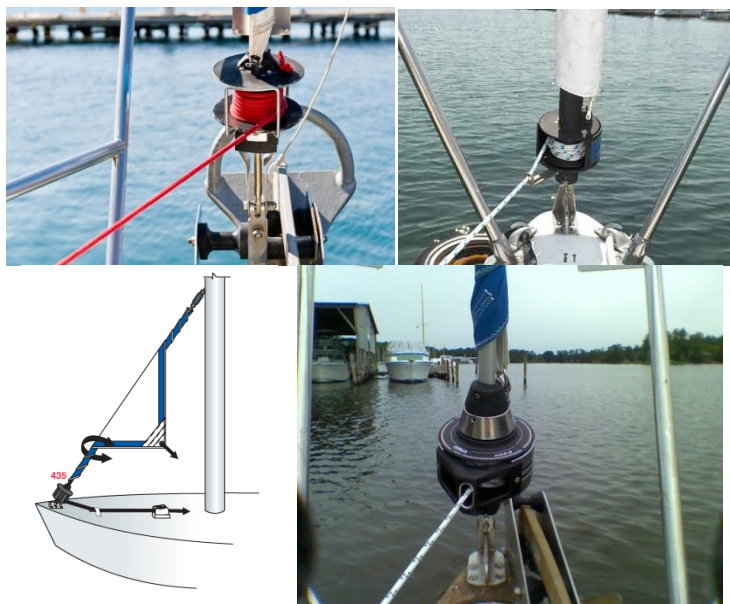


A fully furled roller furling jib and reefed furling mainsail

Jib furling

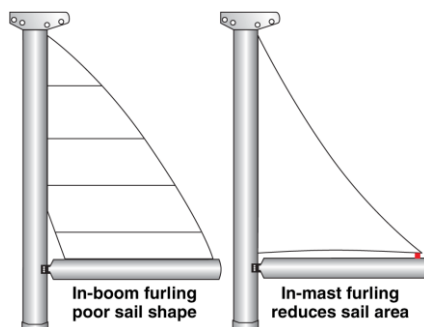
The common furling systems are made for jibs which are attached to the furler with a line around the spool. When the line is pulled the furler turns, rolling up the jib, while when line released the jib sail will be used to unfurl the jib.





Mainsail furling

The mainsail may be furled into the boom which is the more common or to the mast. In the first case the furling system consists of a boom that can rotate along the axis with a latch to lock it in place. To furl the mainsail the boom should be unlocked and then rotated to take up the desired part of the mainsail and then to lock the furler. In advanced boom furling systems the mechanism will put the sail in a slotted cover.

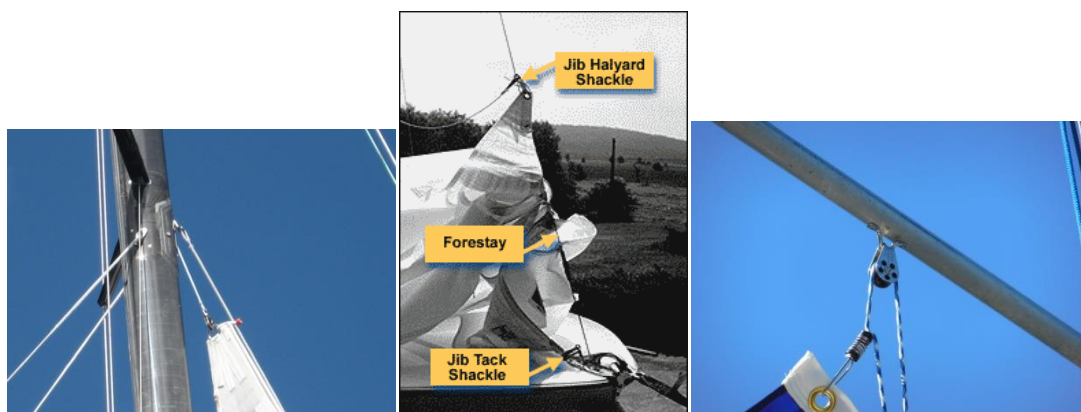
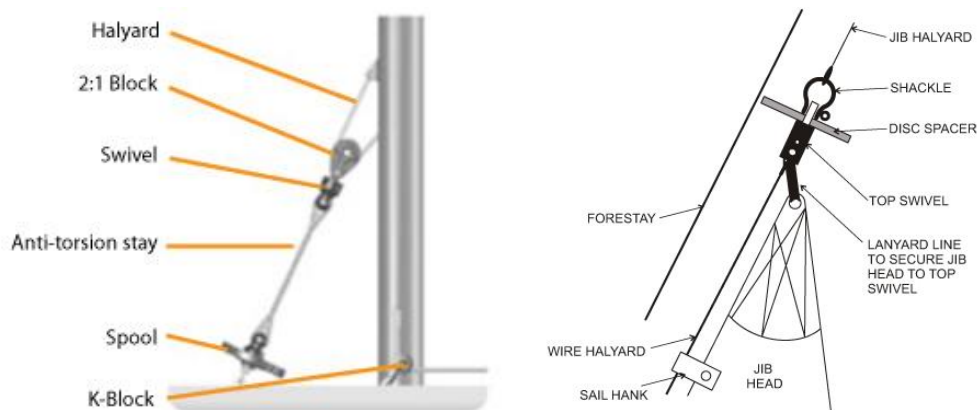


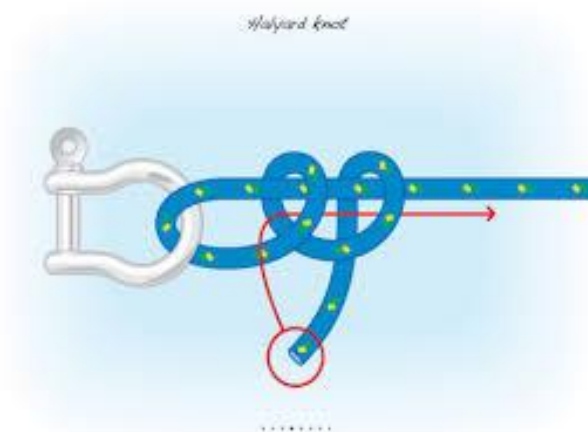


Halyard - Hiliard

This is a line (rope) or wire used to hoist a sail, flag or yard (A yard is a spar on a mast from which sails are set. It may be constructed of timber or steel or from more modern materials like aluminum or carbon fiber reinforced thermoplastic)

As most other parts of the running rigging, they are classically made of natural fiber like manila rope (Manila hemp is a type of fiber obtained from the leaves of the abaca) or hemp fiber.



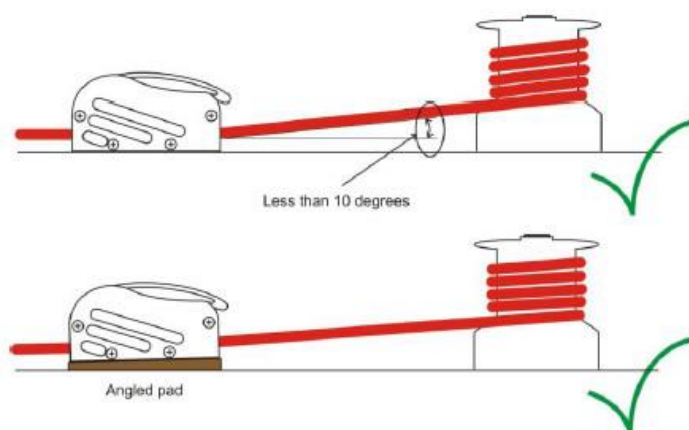


Halyard hangers

Halyard Clutch – Halyard Stopper

Rope clutches are an important part of today's racing sailboat and even cruising sailboats. They allow us to lock off a halyard for the main or jib and even spinnaker without the need for a winch to be taken up. The clutch holds the line firm yet allows for quick and easy adjustment when needed. Rope clutches are available in several rope diameters from 4-14mm. They are also available in one, two, or three combinations. They can also be found for horizontal mounting. Rope Clutches can be used for not just halyards but for tack Lines, Furler Lines (Roller Furling Jibs) and any line that needs to be cleated.





So now we have a rope clutch that will hold the line fast, allow easy trimming, and yet gives quick release under load.

Halyard winch

Sailing winches are almost always taken for granted, yet these compact pieces of mechanical machinery are incredibly powerful. Even the winches on a 30- or 40-foot sailboat are capable of pulling 2 tons or more. The thing about winches is that they're so reliable that most sailors rarely pay much attention to them—until something goes wrong. Using sailing winches correctly should always be a priority.



Jackline

A line, strap, or wire secured from the bow to the stern over the deck as an attachment point for clipping a safety harness, to permit a crewmember to move on the deck with maximum safety, in case of high risk due to heavy weather conditions as well as in time of reduced visibility as at night or during fog.



Double Safety Hook

Generally the jacklines run from the bow towards the stern on deck of both starboard and the port side of the yacht, attached to strong pad eye or cleat



Pad eye

Cleat

Port and **starboard** are nautical and aeronautical terms for left and right, respectively. Port is the left-hand side of a vessel or aircraft, facing forward. Starboard is the right-hand side, facing forward. Since port and starboard never change, they are unambiguous references that are not relative to the observer.

The Jackline are made of low-stretch rope or wire and high strength nylon webbing (is a strong fabric woven as a flat strip or tube of varying width and fibres often used in place of rope).



Rope

Nylon webbing



Lifeline

Lifeline is a wire, cable or netting that runs along the outside of the deck supported by stanchions to help restrain passengers.



Article & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant

ADVERTISING

Antifouling Yachts & Motor boats care & maintenance

What is Antifouling? Why Use an Antifouling?

Antifouling is an important part of care & maintenance and prevents the build-up of harmful organisms and marine plant life from attaching itself to the hull of a yacht or boat.

There are over 4000 different fouling organisms throughout the world and the severity of the fouling is dependent upon where you are sailing and the temperature of the water.

Fouling can take the form of either slime/bio film, or slightly worse seaweed and algae. The most serious type of fouling is Macro fouling, which can be seen as either Hard (mussels, barnacles, tube-worms etc) or Soft (hydroids, sponges, anemones etc).

It's important to antifoul your boat for several different reasons. Firstly, antifoul prevents the transfer by boats of potentially invasive species from one region to another, and therefore reduces the threat to local ecosystems. Secondly, the increased drag and hull friction caused by heavy fouling has a detrimental effect on fuel economy. Good quality antifoul can reduce fuel burn by up 30% for motor boats with heavily fouled hulls and significantly improve speed, manoeuvrability and efficiency for sailing yachts.

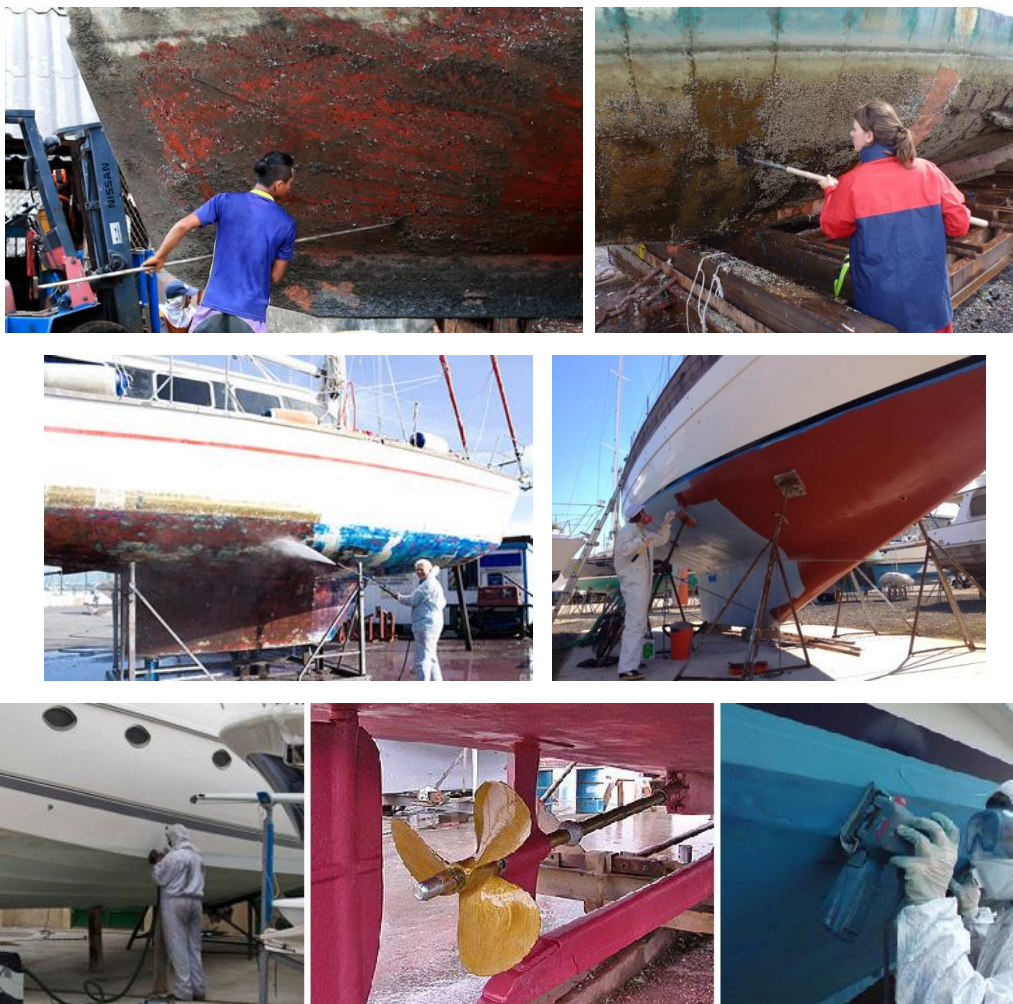


Protect yourself with the wearing of Personal Protective Equipment (PPE):

Because of the hazardous nature of most antifoul coatings, when carrying out the 'DIY' application of your own antifoul, the wearing of Personal Protective Equipment (PPE) is essential.

As a minimum, you will need a pair of coveralls, eye protection, a face mask, a hat or hood and gloves. Sturdy workboats are also required and when working with solvent based paints, cartridge respirators are also recommended.

For further information on 'Safe Antifouling' visit: <http://www.coatings.org.uk/antifouling.aspx>



Practical tips and helpful advice on antifouling your own boat:

Forethought and good preparation is the key, so make sure you read and understand the paint labelling, data sheets and product information before starting.

Observe health & safety recommendations at all times and check whether your chosen boatyard has a 'bundled catchment area', to capture traces of old antifouling and washings in a safe and environmentally responsible way.

You should antifoul outdoors in a safe and secure location, not in a busy or public place such as a car park.

With the boat lifted out of the water and any old fouling removed by jet-wash or scrubbing, you should locate it on a hard-standing and use a suitable tarpaulin, or cover, to capture any scrapings.

Choose a dry day, with as little wind as possible, and try to avoid early morning or early evening when dew might be problem.

Ensure pets, children or bystanders do not come close to the working area and avoid food or drink whilst working.

Inspect the hull for any obvious signs of wear and treat any serious issues, such as osmosis, professionally, before proceeding

Any loose or flaking antifoul can be scraped or wet-sanded back with 60 or 80 grit wet and dry paper, but dry-sanding is not recommended.

If you want to completely remove several layers of old antifouling then gelcoat-friendly chemical removers are useful, as is professional soda-blasting, but never use a blow lamp or heat gun.

Scrub the waterline and then rinse the hull thoroughly with fresh water.

After rubbing back to remove any loose or flaking antifoul, any bare patches can be patch-primed with a suitable primer.

Confirm the correct compatibility of your new paint system with the old coating by checking with the manufacturers data.

Often a full barrier coat or suitable primer coat will avoid doubt.

Mask off the waterline, protect external fittings such as anodes and mask up a clear edge around metal shafts and stern-drives to prevent the risk of a galvanic reaction.

Ensure the coating is kept at room temperature, or as near as possible, before use. Stir well and pour in to a paint tray before re-securing the lid.

To ensure a suitably thick coating is applied, use a medium-pile mohair roller which will help avoid slipping across the hull.

Brush application is also OK, but do not spray antifouling paints.

Load the roller or brush fairly generously, and use a small brush where you need a higher degree of accuracy, such as on the waterline.

Make sure you apply extra coverage on areas such as the leading edge of your keel and rudder and just beneath the waterline.

For the second and successive coats, follow the same procedures as before and observe the correct drying-interval times between coats, and the safe-to-re-launch period indicated on the tin or in product data-sheets.

Watch Seajet's instructional video on 'How to Apply Antifouling' at: <http://www.seajetpaint.com/en/paint-guide/antifoul-your-boat-video/>

Focus on Seajet antifouling paints:

Seajet is owned by Chugoku Marine Paints Ltd (CMP), a company listed on the Japanese Nikkei stock market and one of the world's largest manufacturers and suppliers of paint and protective coatings to commercial shipping and other areas of industry.

Japanese technology leads the world in the science and technology behind protective marine coatings and in March this year, CMP finalised investment in an all new, fully automated, eco-friendly 'state of the art' production facility in Heijningen (20km south of Rotterdam), Netherlands, offering double output capacity when compared to the old factory it now replaces.

Seajet 'self-polishing' antifoul products:

Seajet's best-selling antifoul products comprise mainly self-polishing, copper based formulations, including 031 Samurai, 033 Shogun, 034 Emperor, 037 Coastal, 039 Platinum.

Once the coating comes in to contact with water, a 'chemical reaction' triggers the gradual release of a copper and co-biocide formulation contained within a resin-like substance. By the process of water erosion, the coatings slowly dissolve over time constantly ensuring a fresh layer of coating is always revealed and protecting the hull at all times.

The process of erosion is also known as the 'Leaching Rate', and it's the performance and science behind the control of the 'Leaching Rate' that makes Seajet antifoul so successful.

Other popular products include a 'foul-release' silicon based formulation called Seajet PellerClean and a 'self-polishing' product called Seajet Triple Pack – both of which are designed for protecting propellers and out-drives from fouling.

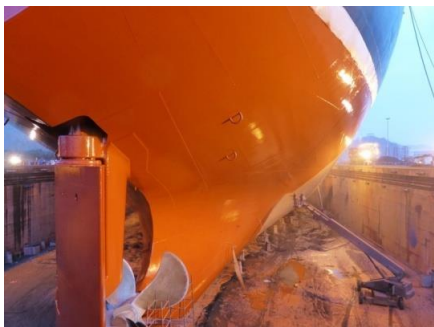
Seajet 038 Taisho eco-friendly alternative:

Seajet have introduced **038 Taisho** – an entirely copper-free formulation that uses ECONEA® as the active ingredient of choice.



A direct result of its low water solubility and low leaching (eroding) rate, **038 Taisho** is a self-polishing, multi-season performer that proves it's now possible to apply a high performance, low biocide, 'non-metallic' antifoul with less environmental impact.

Available in five strong, bright colours, including White, Mid-Blue, Navy-Blue, Red and Black and 2.5L, 5.0L and 20Litre containers, **Taisho** has the benefit of compatibility across all hull types, including polyester, wood, steel and even aluminium.



For news and technical information, and contact details for our 40+ worldwide distributor network, visit our website at: www.seajetpaint.com

Available for most smart devices, Seajet has developed a free to download App offering practical advice on what Seajet antifoul product to use, how to apply it and how much quantity is required for your size and type of boat.

<https://play.google.com/store/apps/details?id=com.seajetpaint.android>

Chugoku Paints BV
Sluisweg 12, 4794 SW, Heijningen, The Netherlands
Tel: +31 (0)167 526 100 Fax: +31 (0)167 522 059
Email: Seajet@cmpeurope.eu



**A.Ceyda Tuglu – IYFR Istanbul Fleet
Seajet Turkey**

Tel: [+ 90 216 701 1172](tel:+902167011172)
Mob: [+ 90 533 221 9054](tel:+905332219054)
actuglu@cmpeurope.eu



You Tube
How to apply Seajet Antifoul



Editing & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant



EDITOR'S NOTE

Dear lyforians friends,

For the last two years 2015-2017 I had taken on my shoulders and in my daily thoughts the task of Rotafloat Editor, being asked and offered to perform the Rotafloat Newsletter by my estimated friend Jesus "Jun" Avecilla, the IC Commodore.

I had taken the challenge with great enthusiasm and strong believe I could perform our Rotafloat Newsletter with a new vision of how to make our Newsletter a true vivid Magazine for our International Yachting Fellowship of Rotarians.

I had kept with my promise and moral obligation to give "birth" to our Rotafloat Newsletter every three month, promise which had been maintained. This is our eight's Issue having started with Issue 99 ending with this 106 Issue.

The actual Rotafloat Newsletter Magazine had passed through a huge change from whatever it was known in the past years. We had performed modern graphic design with a clear beautiful layout including graphical signs related to article's subject, while giving solution for easy table of contents scrolling articles.

The Rotafloat Newsletter is by now a true Magazine publication which is easy to read by specified articles and then to return to the Table of Contents and follow other articles which can interest the reader.

We have given importance to the International Bridge as well as to the activities in each Area of our IYFR giving importance to events in each Area from AGM to Regattas as well as special meetings, all of which had been documented.

We need to underline that my intent was to increase relationship between Fleets as well single mariners & officers willing to participate in other Fleets activities or Regatta so to breathe our international fellowship, having been in connection mainly with all Region Commodores and with many Fleet Commodores asking for information and for written articles.

I have to admit that the task of Rotafloat Editor is more than hard as the information received is not easy to get from our Fellowship members, besides the many articles I had written dedicating time, thoughts and very many white nights to write, edit and send for layout publishing.

Our Rotafloat Newsletter Magazine had for the first time introduced information about sailing boats and sailing boats terms, making our publication a true sailing Magazine and less what it was in the past years a publication of greet & meet.

For the first time we had decided to introduce articles about Geographical Sea Sites in our three areas, information which was more than simple information, but material to study in case willing to prepare a sailing cruise.

We had published for the first time in Rotafloat Newsletter Sailor songs and Sailing songs which connect our Fellowship activity with so many sailors of the past even two centuries back, and to find our "antecessors" in our reality as free-will yacht and motor boats sailors.

Since our first publication we had published articles about Historical Tall Ships giving a tribute to those who had built and operated the Tall Ships around the world, which are still in service to remind us that sailing was based on wind and only much later moved by the force of engine machine.

Rotafloat Newsletter Magazine had aimed for me to become the voice of our Fellowship which by our days is the oldest Fellowship within the Rotary, having celebrated 70 years since its foundation. I wished to create a networking Magazine and motivate our Fellowship Mariners and Officers to participate in events and sailing events in their Area or in other Area while traveling abroad



We had published articles in Memoriam of friends which had left us, but I do feel we had to do more to preserve in our memory members of our Fellowship, but for this is needed collaboration & information from the Fleets in our International Yachting Fellowship of Rotarians.

Our Rotaflat Newsletter Magazine in my personal opinion as well as by people involved in publication of Magazines is considered as high quality professional work, and as so I find we could use it also for advertising, needing to be consulted by auditors and lawyers, about such decision. Whatever in our last edition I had taken decision to publish adverting material concerned with our Istanbul Fleet member, which is concerned with the maintenance of boats.

In all my past editorial notes I had given a true importance on the various religious festivities celebrated around our globe by our Fellowship members, as for me it was symbol of unity instead of dividedness, effort which had been sent to all our lyforianas for the first time as my personal initiative.

I end the "job" of Rottaflat Newsletter Editor with great satisfaction that I had managed "to fly high" dreaming to publish high quality editions, even though I do know that so many of our Fellowship don't read and follow Rotaflat Newsletter Magazine.

I wish to thank and remember forever Rexa Janina Caroline Empeño from the Philippines which was my editorial dedicated graphic partner for almost two years, whom I and her family had lost, having left us all in agony and deep sorrow. I wish deeply to thank Jayson Herrero from the Philippines who was appointed by Jun Avecilla to take Rexa's job, to be so helpful with the last two Rotaflat Newsletter publications, and appreciation to Raffy Mayor Rabara, graphic artist.

I wish to thank my estimated friend Jesus "Jun" Avecilla, the IC Commodore for his grate support and believe in my hard working job for which I had dedicated my time spread during days and nights, being active in my professional work as architect.

I deeply appreciate the recognition plate received in Atlanta during the Gala Dinner with the following dedication:



In grateful recognition for his Invaluable Service to the
International Yachting Fellowship of Rotarians
As International Staff and Rotaflat Editor for the Years 2015-2017
Creating an innovative Newsletter Rotaflat published quarterly

This token of appreciation is given to

Zeev Matar

At the AGM and Change of Command Ceremonies,
Atlanta, Georgia, June 13, 2017.



Jesus C Avecilla
International Commodore
2015-2017



I will end my Editor's note with publishing my lyforian poem dedicated to our Israeli Fellowship in Hebrew my home language and in Italian which is my second acquired language and add my poem translated to Greek, having accomplished with dear friends the foundation of the Hellenic Ionian Fleet and the Hellenic Aegean Fleet.

I wish to my successor success in taking the task of Rotafloat Editor, keeping with the quality of the Magazine I had managed to create for our International Yachting Fellowship of Rotarians.

**TO ALL IYFR FELLOWSHIP MEMBERS
BEST WISHES FOR HAPPY SUMMER OR WINTER
HOLIDAYS WISHING FOR EACH OF YOU AS WELL AS FOR
YOUR DEAR FAMILIES
HEALTH, HAPPINESS AND JOY
WITH OPEN SAILS AND BLOWING WIND**

Best regards with Ciao and Israeli Shalom to all of you with our Pilipino toast Mabuhay



Picture taken during Atlanta Annual General Meeting 13.6.2017

Zeev

**Article & Layout
Zeev Matar**



Rotafloat Magazine Editor / IC Special Assistant





International Yachting Fellowship of Rotarians



Dedicated poem to Our Fellowship in Israel

שיר לשייטי אגודת היאכטות של הרוטרי הבינלאומי

אגודת היאכטות של הרוטרי
אנו מלכי הימים
חוצים כל אגם או אוקינוס
ברוח הנושבת במפרשים
כולנו ציים מאוחדים
פרושים תחת דגלוננו ציים שונים
בכל מדינה
עושים אהבה עם כל הימים
קדימה שייטו חברים
במפרשי ידידות פתוחים
הידידות היא שפתינו
וודקה ושמפיין
גבעינו הנצחיים
יין ורום
הם דמנו הזורם בעורקים
תחי החברות
לחיי כולכם חבריי
שלוח לכל חברי אגודת היאכטות
שלום היא המילה, אמן
המשיכו לשוט ימאים
זכו עבורינו בגביעים
שיט היאכטות אהבת כולנו
אנחנו שייטים מסורים
ביום סגריר או באור השמש
קדימה לשוט
ימאים וקצינים
הבה נמצא אין סוף שמחה
תחי אגודת היאכטיונרים של הרוטרי
באו נזיז קדימה הגה הספינה
ונזכה בברכת פוסידון היוני
לאתגר את היאכטות שלנו ואת הרוחות
ולתת בידינו תמיד ליזכות

זאב

Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant





SAILOR SONGS



White Wings

**Sail! home, as straight as an arrow,
My yacht shoots along on the crest of the sea;
Sail! home, to sweet Maggie Darrow,
In her dear little home
She is waiting for me.**

**High up! Where cliffs they are craggy
There's where, the girl of my heart waits for me
Heigh! ho, I long for you, Maggie
I'll spread out my "White Wings"
And sail home to thee.**

**Yo! ho, how we go!
Oh! how the winds blow!
"White Wings" they never grow weary,
They carry me cherrily over the sea.
Night comes, I long for my dearie,
I'll spread out my "White Wings"
And sail home to thee.**

White Wings was a popular novel written by William Black in the 1880's. In 1912 Banks Winter wrote this song. "White Wings" refers to the sails of a ship



SAILING SONGS



White Sails

**White sails... White sails...
Mast running high,
run and knock on every door
tell the women not to cry...**

**Here they come...
I can see most everyone,
In the early morning sun
I can hear the sing from men.**

**See how they conquered the wind and the ocean waves...
See how they stand so mighty.
One hundred days
Since they went far beyond the sea
Leaving us an empty chantey...
How many tears did we cried**

**Waiting for this day,
In our dark imagination...
How many hopes of return do they now fulfilled
In this happy celebration.**

**White sails... White sails...
Mast running high,
run and knock on every door
tell the women not to cry...**

**Here they come...
I can see most everyone,
In the early morning sun
I can hear the sing from men.**



**In marry churches bells of echo in every home...
Spread the good news around you.**

**Tell everyone that the men have at last returned...
And that every wish has come true.
How many days did we pray for each one of them
In our lonely expectation...
How many hopes of return do they now fulfilled
In this happy celebration.**

**White sails... White sails...
Mast running high,
run and knock on every door
tell the women not to cry...**

**Here they come...
I can see most everyone,
In the early morning sun
I can hear the sing from men.**

**Music: Dimos Moutsis
English Lyrics: Demis Roussos
Performed by: Demis Roussos**



Demis Roussos – White sails



Demis Roussos Concert Bratislava



White sails

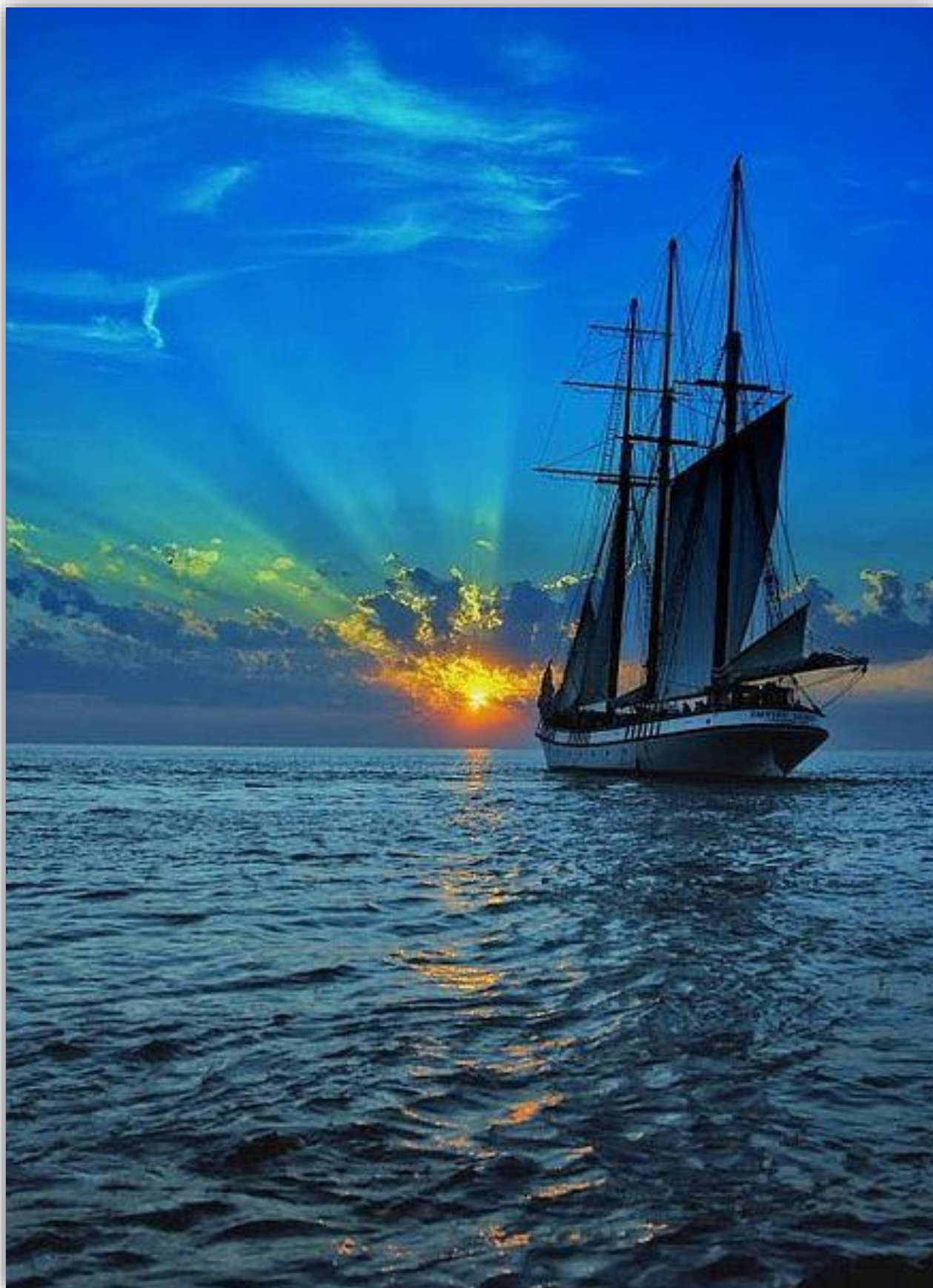














Historical Tall Ships

KRUZENSHTERN 1927



Class A -4 mast Barque **The Largest traditional sailing vessel still in operation**

The Kruzenshtern is a four mastered barque built in 1926 in Geestemünde (Geeste river running through Lower Saxony and Bremen) in Bremerhaven, Germany. The Tall ship was built under the name of Padua the Italian city of Padova in northern Italy (the capital of the province of Padua located in the area of Venice)

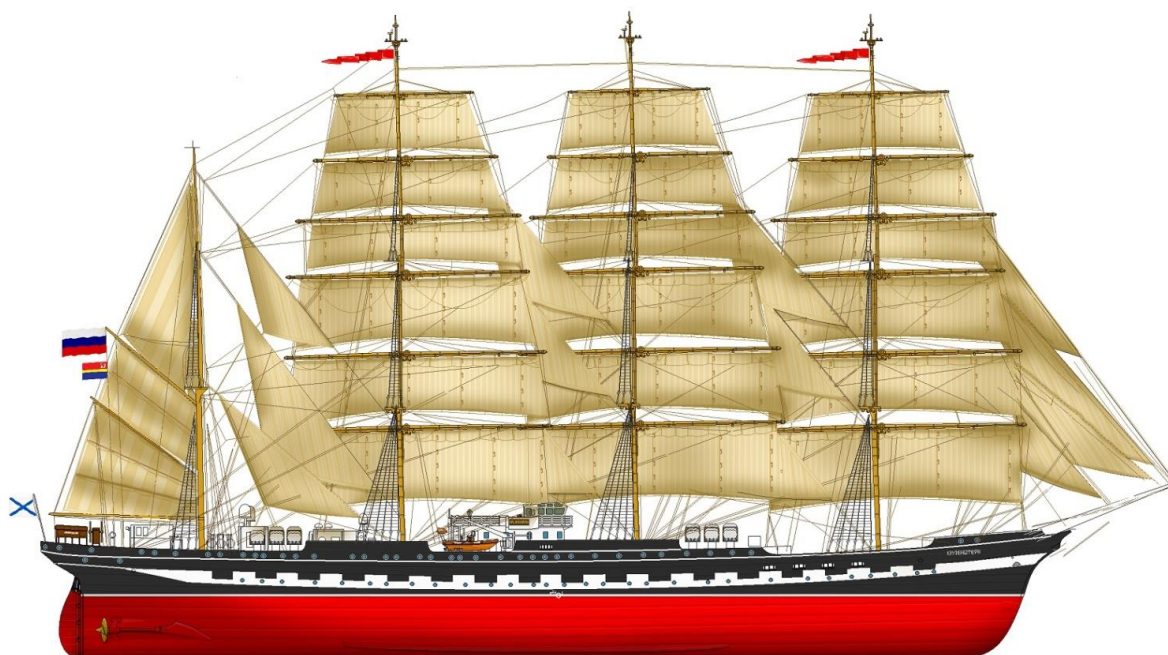
The vessel is 95 meters length excluding bowsprit with beam of 14 meters, which acknowledge this vessel as the largest traditional sailing vessel still in operation

By definition all square-rigged vessels (barque, barquentine, brig, brigantine or ship rigged) with all square-rigged vessels with LOA – Length Overall measured from the fore side of stern post to aft side of stern post, with more than 40 meters.



Padua was launched in 1926 as the last tall ship of the Flying P-Liners still in use which was built by the shipping company of F. Laeisz from Hamburg in Germany founded in 1824 as a hat manufacturing company turned into shipping business. From mid 1880 on all their ships names had started with “P” from the nickname of Carl’s Laeisz wife “Pudel”.

Like all P-liners, Padua was painted according to the colors of the German national flag of the German Empire: Black (hull above water and topsides), White (waterline area), and Red (underwater body).



WIP 26-12

Kruzenshtern - Kaliningrad

Padua - Kruzenshtern is one of the four remaining Flying P-Liners, and as mentioned is the only one still in use mainly as training tall ship moored at her home ports of Kaliningrad and Murmansk.

Padua was commissioned as a cargo ship to transport construction materials to Chile in South America. Returning with *saltpeter around Cape Horn.

*

Sodium nitrate is the chemical compound NaNO_3 also known as Peru saltpeter found in the Atacama Desert which is used on industrial scale for production of fertilizers, glass production, pottery enamels and others.

Later this tall ship had transported wheat from Australia.

Record voyages

In 1933-1934 she took a record-breaking 67 days from Hamburg, Germany to Port Lincoln in South America.

Before World War II she made 15 long trips to Chile and Australia.

Her fastest voyage from Hamburg – Chile – Australia – Hamburg was performed in 8 months and 23 days which is a World Record Voyage for tall ships that has never been broken

On 12/1/1946 the Kruzenshtern was to the USSR and integrated into the Baltic Fleet of the Soviet Navy. The tall ship had moored until 1961 in Kronstadt harbor where she had gone for repairs as well as installation of her first engines, becoming part of the Hydrographic Department of the Soviet Navy.

From 1961 to 1965 the Kruzenshtern was under service of many hydrographic and oceanographical surveys for the Academy of Science of the USSR, as well as being used as a training vessel for naval cadets.

In 1965 the vessel had been transferred to the USSR Ministry of Fisheries in Riga to be used as a training school ship for future fishery officers.

The Kruzenshtern had gone from 1968 – 1972 through modernization with installation of set of engines as well as painting as it was painted originally.

In January 1981 the vessel was transferred to the Estonian Fisheries Industry at Tallinn and in 1991 she had become part of the “State Baltic Academy of the Fisheries” fleet in her new port home of Kaliningrad

The General characteristics of the Kruzenshtern are:

Length: 114,4 m (375 ft)

Height: 51,3 m (168 ft)

Beam: 14.02 (46.0 ft)

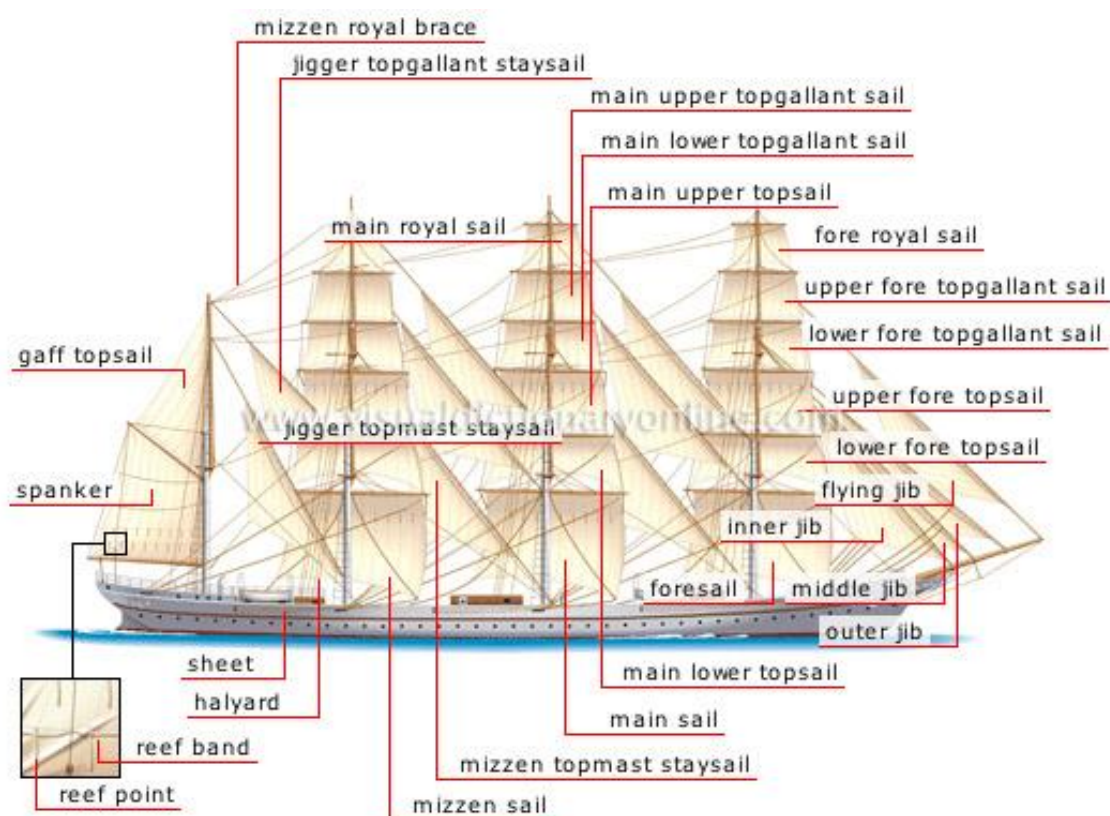
Draft: 6.8 m (22 ft)

Installed power: 2x1000bhp 8 cylinder Diesel engine

Propulsion: sails + Diesel engine

Complement: 60 people (25 crew + 35 trainees) up to 257 sailing people.

Tonnage: 3,064 GRT as Padua in 1927



Baron **Ivan Fyodorovich Kruzenshtern** (10 October 1770 – 12 August 1846), born as **Adam Johann Ritter von Krusenstern**, was a Russian Admiral and explorer of Baltic German family descended from Swedish aristocratic family. In 1787, he joined the Russian Imperial Navy, and served in the war against Sweden. Subsequently, he served in the Royal Navy in 1793–1799, visiting America, India and China.



Krusenstern led the first Russian circumnavigation of the world. The chief object of this undertaking was the development of the fur trade with Russian America (Alaska). Other goals of the two-ship expedition were to establish trade with China and Japan, facilitate trade in South America, and examine the coast of California in western North America for a possible colony.

The crater Krusenstern on the Moon is named after him. There is Krusenstern Island in the Bering Strait, as well as a small group of islands in the Kara Sea, southwest of the Nordenskiöld Archipelago, called Krusenstern Islands. Cape Krusenstern in Northwest Alaska is the site of Cape Krusenstern National Monument (1978), one of the most important archaeological sites in the state.

We wish to point the legacy the Cook Islands in the South Pacific bear that name thanks to von Krusenstern. Previously known as the Hervey (or Harvey) Islands (or Group), he changed their name in 1835 to honor Captain Cook. More accurately, he changed the name of those which comprised the Southern Group and it was subsequently applied to all 15 islands when the New Zealand Parliament passed "The Cook Islands and other Islands Government Act" in 1901. He recorded the new name in his "Atlas de l'Océan Pacifique" published at St. Petersburg between 1824 and 1835.

The Padua / Kruzenshtern in films

The ship was used in three German films — *Die Meuterei auf der Elsinore* (1935); *Herz geht vor Anker* (1940) and *Große Freiheit Nr. 7* (1944), as well as a number of Russian and Soviet films.

In 1997 she was the main subject of an Estonian/British documentary produced by Allfilm and First Freedom Productions called 'Tall Ship' and transmitted on Discovery. The one-hour programme was directed by Rein Kotov and produced by Graham Addicott and Pille Runk.



Historical names & notes



Germany

Name: Padua
 Namesake: Padua
 Owner: F. Laeiez , Hamburg 1926-1946
 Operator: F. Laeiez , Hamburg 1926-1946
 Builder: Joh.C. Tecklenborg, Bremerhaven
 Yard number: 408
 Port of registry: Hamburg
 Launched: 11.6.1926



USSR

Name: Kruzenshtern
 Namesake: Adam Johann Krusenstern
 Owner: Soviet Navy (1946 – 1991)
 Port of registry: Riga (1946-1981)
 Tallinn (1981-1991)
 Kaliningrad (1991 -)

Acquired: 1946
 Call sign: UCVK



Russia

Name: Kruzenshtern
 Namesake: Adam Johann Krusenstern
 Owner: Soviet Navy (1946 – 1991)
 Kaliningrad (1991 -)

Acquired: 1946
 Call sign: UCVK





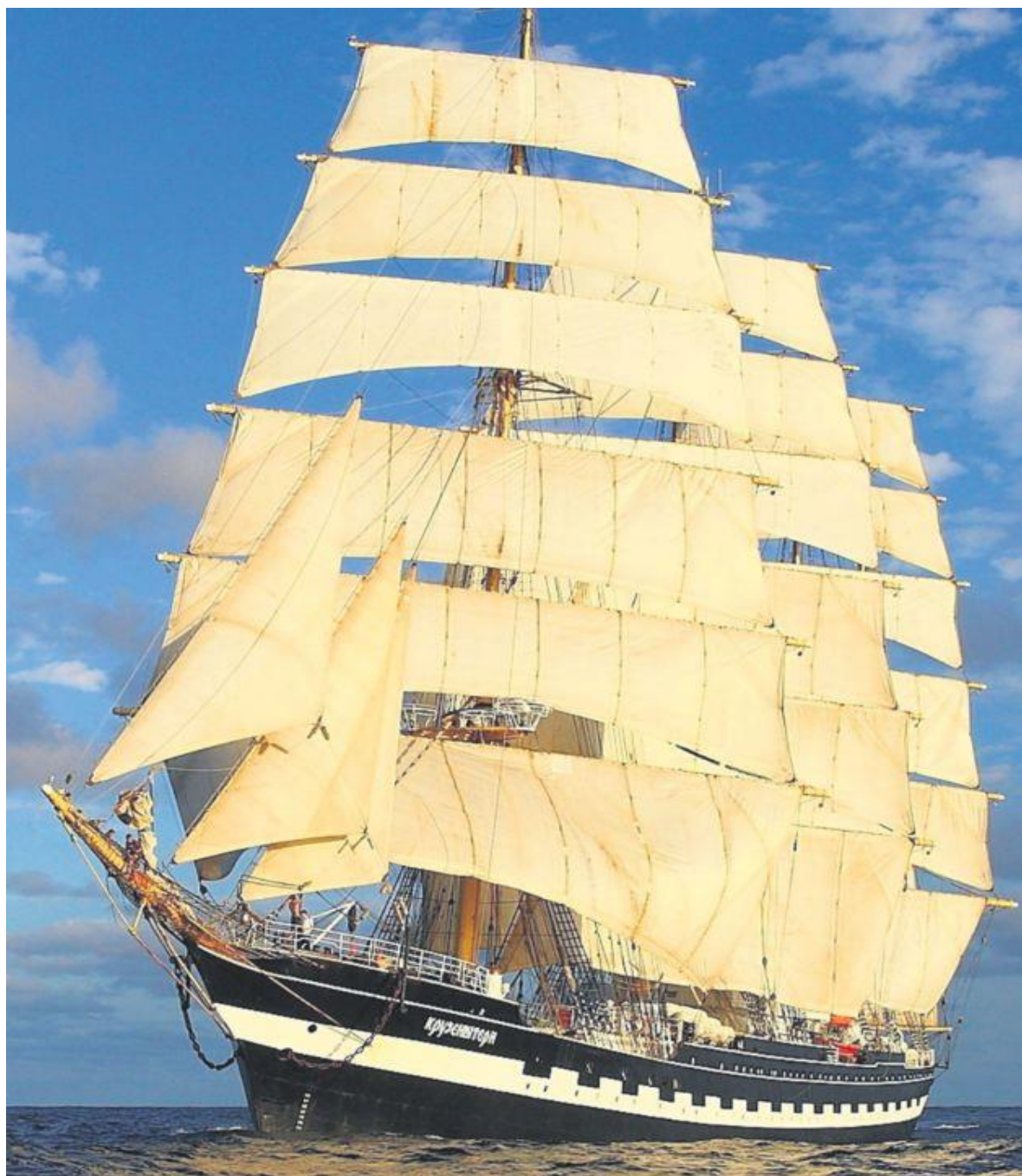












The Kruzenshtern



Kruzenshtern behind the scenes

Article & Layout
Zeev Matar



Rotafloat Magazine Editor / IC Special Assistant